



April 27, 2015

State of Vermont
Southwest Regional Construction Office
Attn: Chris Lavalette, Resident Engineer
61 Valley View
Rutland, VT 05701

Re: Bridport STP CULV (29)

Dear Mr. Lavalette,

Please find the Traffic Control Plan for the above mentioned project.

Phase I BR 2

Message boards will be erected two weeks prior to job start. Per attached traffic control sheet 1 & 2, required signs will be erected and flaggers will be utilized as single lane road closures are needed to install erosion control and temporary water diversion. Prior to the start of the culvert excavation, Route 125 will be closed to traffic and the detour will be utilized. After the box culvert is installed, guardrail and base course pavement are complete, flaggers will be utilized during single lane road closures for the remainder of the work on Bridge 2.

Phase II BR 5

Message boards will be erected two weeks prior to job start. Per attached traffic control sheet 1 & 2, required signs will be erected and flaggers will be utilized as single lane road closures are needed to install erosion control and temporary water diversion. Prior to the start of the culvert excavation, Route 125 will be closed to traffic and the detour will be utilized. After the box culvert is installed, guardrail and base course pavement are complete, flaggers will be utilized during single lane road closures for the remainder of the work on Bridge 2.

The above project phasing will include the following:

Construction Signing: Detour Signs will be placed at locations designated per Detour Plan and Traffic Control Details sheets 18-20. Construction signs will be installed prior to beginning work at the project's first location, Bridge 2 on Route 125. Bridge locations will be posted with "Road Closed 1000 Ft" and "Road Closed" and will remain in place until Route 125 is open for single lane traffic. Once Route 125 is open for traffic, a sign package will be utilized with Windmaster signs consisting of "Road Work Ahead", "Single Lane Ahead", "Flagger", Flag Tree, and "End Road Work". Detour Signs at the second location, Bridge 5 on Route 125 will be placed at locations designated per Detour Plan and Traffic Control Details sheets 43-45 prior to beginning work. Bridge locations will be posted with "Road Closed 1000 Ft" and "Road Closed" and will remain in place until Route 125 is open for single lane traffic. Once Route 125 is open for traffic, a sign package will be utilized with Windmaster signs consisting of "Road Work Ahead", "Single Lane Ahead", "Flagger", Flag Tree, and "End Road Work". Sign packages will meet the requirements of the MUTCD. All sign stands will be NCHRP 350 or MASH compliant. Signs may be erected at the end of May 2015, but will be covered until a minimum of two weeks prior to the commencement of work the week of June 1, 2015 for Bridge 2 and June 29, 2015 for Bridge 5.

Portable Message Boards: Message Boards will be installed at locations per Detour Plan sheets 18, 20 and 43, 45 prior to placement of construction signage. This will occur a minimum of two weeks prior to construction, to provide ample warning to traveling public.

Work Zone Delineation: Work Zones will be delineated using traffic cones or construction drums during single lane closures.

Flagging: Flaggers will be utilized when needed for single lane closures and additional traffic control.

Temporary Striping: Line striping targets and temporary striping will be used to delineate center lines after paving of base and binder courses at each location.

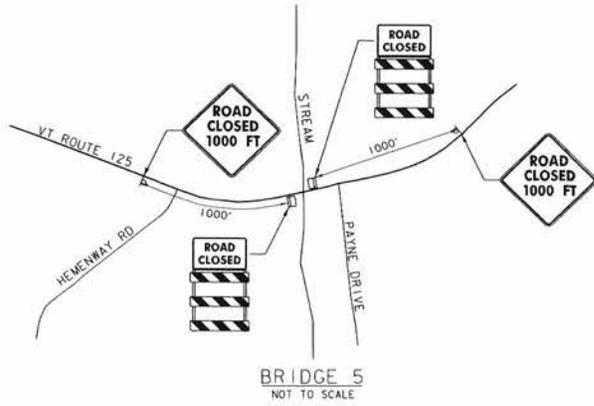
Traffic Control Coordinator: The traffic control coordinator will be Gary Olcott. All decisions regarding traffic control implementation will be addressed by him.

Please refer to the accompanying Traffic Control Sheets for further details. If you have any questions or need any additional information regarding this plan, please contact me at (518) 792-3157 or at mpetr@peckham.com.

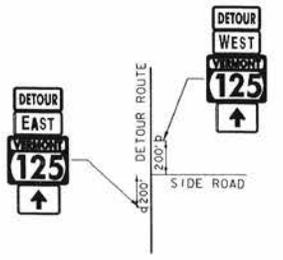
Thank you,

A handwritten signature in black ink, appearing to read "Mark Petramale". The signature is fluid and cursive, with the first name "Mark" being more prominent than the last name "Petramale".

Mark Petramale
Project Manager
Peckham Road Corporation



BRIDGE 5
NOT TO SCALE



CONFIRMATION ASSEMBLY
NOT TO SCALE

PCMS = PORTABLE CHANGEABLE MESSAGE SIGN

ID NUMBER	SIGN TEXT	SIZE OF SIGN		NUMBER OF SIGNS REQ'D.	AREA OF EACH SIGN (SF)	COLOR	REMARKS
		WIDTH	HEIGHT				
W20-2	DETOUR AHEAD	48"	48"	5	16.00	B/F/O	
W20-3	ROAD CLOSED 1000 FT	48"	48"	2	16.00	B/F/O	
M6-3	↑	21"	15"	16	2.19	B/F/O	MOUNT BELOW MI-6A
M6-1R	→	21"	15"	5	2.19	B/F/O	MOUNT BELOW MI-6A
M6-1L	←	21"	15"	3	2.19	B/F/O	MOUNT BELOW MI-6A
M5-1L	↙	21"	15"	3	2.19	B/F/O	MOUNT BELOW MI-6A
M5-1R	↘	21"	15"	4	2.19	B/F/O	MOUNT BELOW MI-6A
M5-2L	↖	21"	15"	1	2.19	B/F/O	MOUNT BELOW MI-6A
M6-2L	↗	21"	15"	1	2.19	B/F/O	MOUNT BELOW MI-6A
M4-8	DETOUR	24"	12"	33	2.00	B/F/O	MOUNT ABOVE M3-2 OR M3-4
M4-8A	END DETOUR	24"	18"	2	3.00	B/F/O	MOUNT BELOW MI-6A
M3-2	EAST	24"	12"	18	2.00	G/W	
M3-4	WEST	24"	12"	15	2.00	G/W	
M1-6B	VERMONT 125	30"	24"	35	5.00	G/W	
R11-2C	ROAD CLOSED	48"	30"	2	10.00	B/W	
R11-3B	ROAD CLOSED 1.5 MILES AHEAD NO THRU TRUCKS	60"	30"	1	12.50	B/W	
R11-3B	ROAD CLOSED 6 MILES AHEAD NO THRU TRUCKS	60"	30"	1	12.50	B/W	

B/F/O = BLACK ON RETROREFLECTIVE ORANGE BACKGROUND
G/W = GREEN ON RETROREFLECTIVE WHITE BACKGROUND
B/W = BLACK ON RETROREFLECTIVE WHITE BACKGROUND

NOTES:

- ALL COSTS OF INSTALLING, MAINTAINING, AND REMOVING THE SIGNS AND BARRICADES IN THIS TRAFFIC CONTROL PLAN AS NECESSARY TO MEET PROJECT CONDITIONS SHALL BE INCLUDED IN ITEM 900.645 SPECIAL PROVISION (TRAFFIC CONTROL, ALL-INCLUSIVE). SEE SPECIAL PROVISIONS.
- ALL TRAFFIC SIGNS SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) 2009 EDITION.
- "ROAD CLOSED" SIGNS SHALL BE MOUNTED AND MAINTAINED ON RETROREFLECTIVE TYPE III BARRICADES.
- TYPE III CONSTRUCTION BARRICADES SHALL BE PLACED SO AS TO PHYSICALLY EXCLUDE TRAFFIC FROM THE ENTIRE ROADWAY WIDTH OR AT THE DISCRETION OF THE ENGINEER.
- WHEN POSSIBLE LOCATE THE "ROAD CLOSED 1000 FT" SIGN NEAR A LOCATION WHERE DRIVERS CAN TURN AROUND.
- THE MI-6B, THE M3-2 AND THE M3-4 SIGNS SHALL BECOME THE PROPERTY OF THE STATE AFTER THEY ARE REMOVED FROM THE DETOUR. THE CONTRACTOR SHALL DELIVER THE SIGNS TO THE STATE AT THE DISTRICT #5 GARAGE.
- WHERE POSSIBLE LOCATE DETOUR ROUTE MARKER ASSEMBLIES ADJACENT TO EXISTING ROUTE MARKER ASSEMBLIES.
- SIGN SPACING IS FOR REFERENCE ONLY, FIELD ADJUSTMENTS AS APPROVED BY THE ENGINEER.
- EXISTING SIGNS IN CONFLICT WITH THIS DETOUR PLAN SHALL BE COVERED WHEN NECESSARY, AS APPROVED BY THE ENGINEER.

MESSAGES FOR PORTABLE CHANGEABLE MESSAGE SIGNS (PCMS) - REGIONAL DETOUR

STARTING 2 WEEKS PRIOR TO CLOSURE

ROUTE	MESSAGE 1	MESSAGE 2	(DATE)
**	VT 125 ROAD CLOSED	MMMM DD TO MMMM DD	**

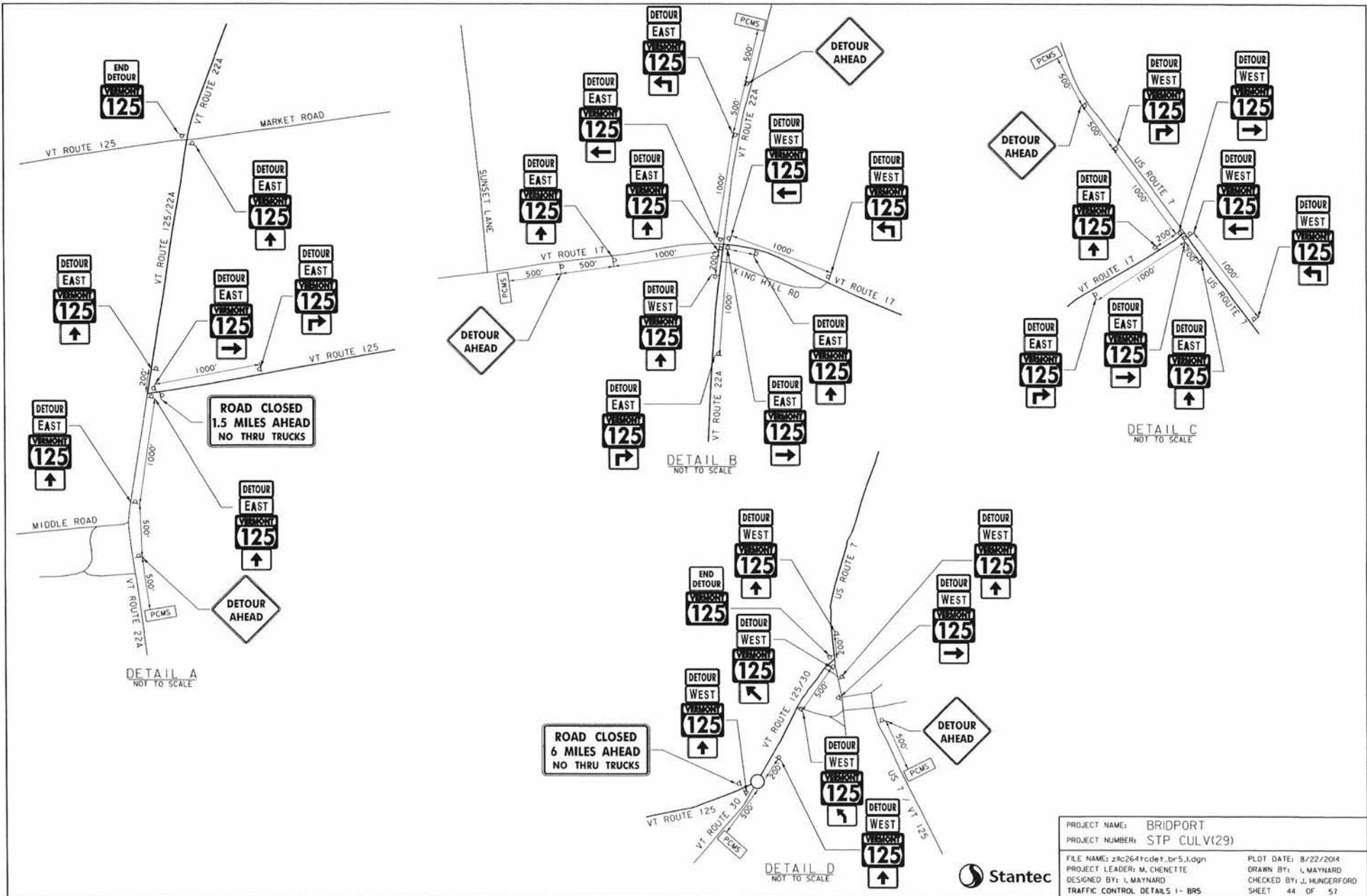
DURING CLOSURE

ROUTE	MESSAGE 1	MESSAGE 2
**	VT 125 ROAD CLOSED	BRIDPORT 1 MILE E OF VT 22A

- DATE SHALL BE SPELLED OUT (I.E. JUNE 10 NOT 6/10)
- ROUTE 125 SHALL SPECIFY E (EAST) OR W (WEST) AS APPROPRIATE FOR THE DETOUR.



PROJECT NAME: BRIDPORT	PLOT DATE: 8/22/2014
PROJECT NUMBER: STP CULV(29)	DRAWN BY: I. MAYNARD
FILE NAME: zllc264tcdet_br5.2.dgn	CHECKED BY: J. HUNGERFORD
PROJECT LEADER: M. CHENETTE	SHEET 45 OF 57
DESIGNED BY: I. MAYNARD	
TRAFFIC CONTROL DETAILS 2 - BR5	



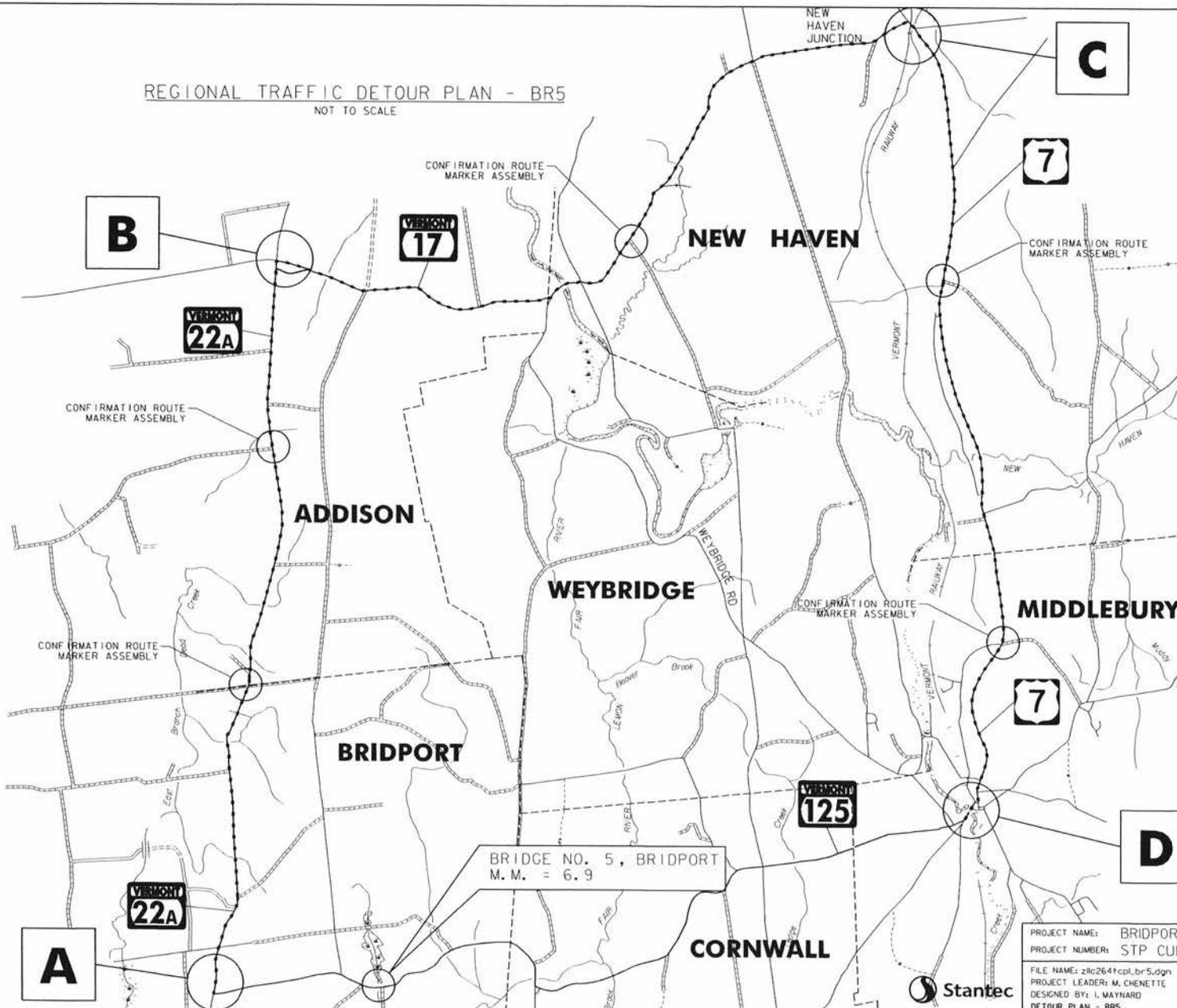
PROJECT NAME:	BRIDPORT	PLT DATE:	8/22/2014
PROJECT NUMBER:	STP CULV(29)	DRAWN BY:	L. MAYNARD
FILE NAME:	z8c264fcdet1.br5.ldgn	CHECKED BY:	J. HUNGERFORD
PROJECT LEADER:	M. CHENETTE	SHEET	44 OF 57
DESIGNED BY:	L. MAYNARD		
TRAFFIC CONTROL DETAILS I - BR5			





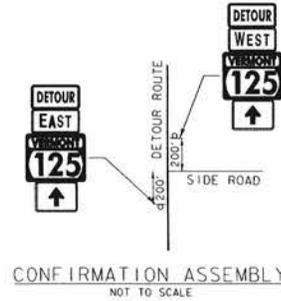
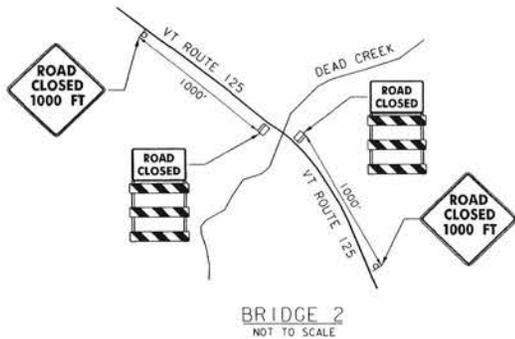
REGIONAL TRAFFIC DETOUR PLAN - BR5
NOT TO SCALE

----- DETOUR ROUTE



PROJECT NAME:	BRIDPORT	PLOT DATE:	8/22/2014
PROJECT NUMBER:	STP CULV(29)	DRAWN BY:	I. MAYNARD
FILE NAME:	zllc2641cpl.br5.dgn	CHECKED BY:	J. HUNGERFORD
PROJECT LEADER:	M. CHENETTE		
DESIGNED BY:	I. MAYNARD		
DETOUR PLAN - BR5			SHEET 43 OF 57





NOTES:

1. ALL COSTS OF INSTALLING, MAINTAINING, AND REMOVING THE SIGNS AND BARRICADES IN THIS TRAFFIC CONTROL PLAN AS NECESSARY TO MEET PROJECT CONDITIONS SHALL BE INCLUDED IN ITEM 900.645 SPECIAL PROVISION (TRAFFIC CONTROL, ALL-INCLUSIVE). SEE SPECIAL PROVISIONS.
2. ALL TRAFFIC SIGNS SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) 2009 EDITION.
3. "ROAD CLOSED" SIGNS SHALL BE MOUNTED AND MAINTAINED ON RETROREFLECTIVE TYPE III BARRICADES.
4. TYPE III CONSTRUCTION BARRICADES SHALL BE PLACED SO AS TO PHYSICALLY EXCLUDE TRAFFIC FROM THE ENTIRE ROADWAY WIDTH OR AT THE DISCRETION OF THE ENGINEER.
5. WHEN POSSIBLE LOCATE THE "ROAD CLOSED 1000 FT" SIGN NEAR A LOCATION WHERE DRIVERS CAN TURN AROUND.
6. THE MI-6B, THE M3-2 AND THE M3-4 SIGNS SHALL BECOME THE PROPERTY OF THE STATE AFTER THEY ARE REMOVED FROM THE DETOUR. THE CONTRACTOR SHALL DELIVER THE SIGNS TO THE STATE AT THE DISTRICT #5 GARAGE.
7. WHERE POSSIBLE LOCATE DETOUR ROUTE MARKER ASSEMBLIES ADJACENT TO EXISTING ROUTE MARKER ASSEMBLIES.
8. SIGN SPACING IS FOR REFERENCE ONLY. FIELD ADJUSTMENTS AS APPROVED BY THE ENGINEER.
9. EXISTING SIGNS IN CONFLICT WITH THIS DETOUR PLAN SHALL BE COVERED WHEN NECESSARY, AS APPROVED BY THE ENGINEER.

MESSAGES FOR PORTABLE CHANGEABLE MESSAGE SIGNS (PCMS) - REGIONAL DETOUR

STARTING 2 WEEKS PRIOR TO CLOSURE

	MESSAGE 1	MESSAGE 2	
(ROUTE) **	VT 125	MMMM DD	(DATE) *
	ROAD	TO	
	CLOSED	MMMM DD	(DATE) *

DURING CLOSURE

	MESSAGE 1	MESSAGE 2
(ROUTE) **	VT 125	EAST OF
	ROAD	BASIN
	CLOSED	HARBR RD

- * - DATE SHALL BE SPELLED OUT (I.E. JUNE 10 NOT 6/10)
- ** - ROUTE 125 SHALL SPECIFY E (EAST) OR W (WEST) AS APPROPRIATE FOR THE DETOUR.

PCMS = PORTABLE CHANGEABLE MESSAGE SIGN

ID NUMBER	SIGN TEXT	SIZE OF SIGN WIDTH HEIGHT	NUMBER OF SIGNS REQ'D.	AREA OF EACH SIGN (SF)	COLOR	REMARKS
W20-2	DETOUR AHEAD	48" 48"	7	16.00	B/F/O	
W20-3	ROAD CLOSED 1000 FT	48" 48"	2	16.00	B/F/O	
M6-3	↑	21" 15"	13	2.19	B/F/O	MOUNT BELOW MI-6A
M6-1R	→	21" 15"	4	2.19	B/F/O	MOUNT BELOW MI-6A
M6-1L	←	21" 15"	4	2.19	B/F/O	MOUNT BELOW MI-6A
M6-2L	↖	21" 15"	1	2.19	B/F/O	MOUNT BELOW MI-6A
M5-1L	↙	21" 15"	4	2.19	B/F/O	MOUNT BELOW MI-6A
M5-1R	↘	21" 15"	4	2.19	B/F/O	MOUNT BELOW MI-6A
M5-2L	↗	21" 15"	1	2.19	B/F/O	MOUNT BELOW MI-6A
M4-8	DETOUR	24" 12"	31	2.00	B/F/O	MOUNT ABOVE M3-2 OR M3-4
M4-8a	END DETOUR	24" 18"	2	3.00	B/F/O	MOUNT BELOW MI-6A
M3-2	EAST	24" 12"	18	2.00	G/W	
M3-4	WEST	24" 12"	13	2.00	G/W	
MI-6B	VT 125	30" 24"	33	5.00	G/W	
R11-2C	ROAD CLOSED	48" 30"	2	10.00	B/W	
R11-3B	ROAD CLOSED 2 MILES AHEAD NO THRU TRAFFIC	60" 30"	2	12.50	B/W	

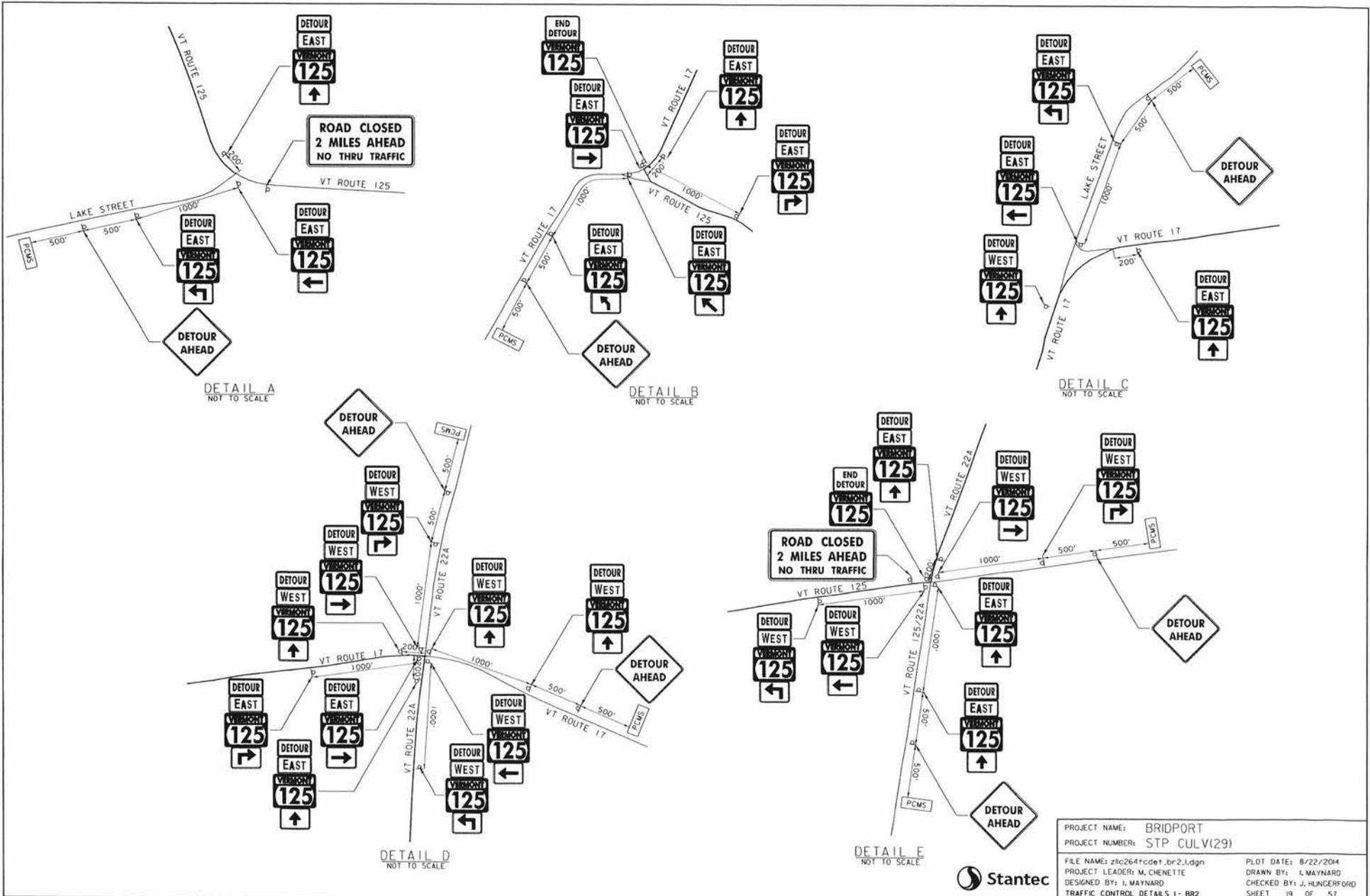
B/F/O = BLACK ON RETROREFLECTIVE ORANGE BACKGROUND
G/W = GREEN ON RETROREFLECTIVE WHITE BACKGROUND
B/W = BLACK ON RETROREFLECTIVE WHITE BACKGROUND



PROJECT NAME: BRIDPORT
PROJECT NUMBER: STP CULV(29)

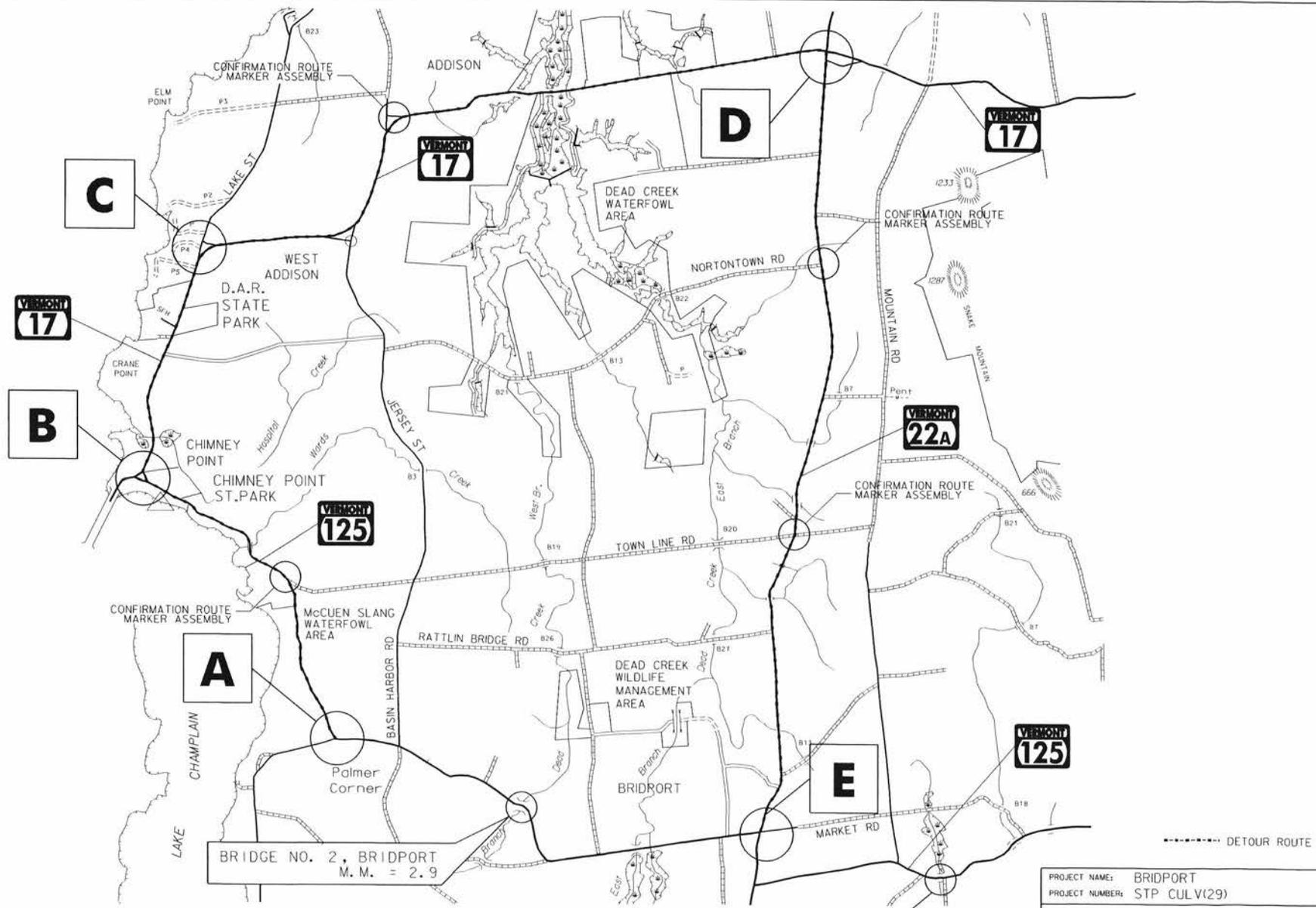
FILE NAME: zllc264fcdet.br2.2.dgn
PROJECT LEADER: M. CHENETTE
DESIGNED BY: L. MAYNARD
TRAFFIC CONTROL DETAILS 2 - BR2

PLOT DATE: 8/22/2014
DRAWN BY: L. MAYNARD
CHECKED BY: J. HUNGERFORD
SHEET 20 OF 57



PROJECT NAME:	BRIDPORT	PLDT DATE:	8/22/2014
PROJECT NUMBER:	STP CULV(29)	DRAWN BY:	L.MAYNARD
FILE NAME:	zllc264+rdet_br2.dgn	CHECKED BY:	J.HUNGERFORD
PROJECT LEADER:	M.CHENETTE	TRAFFIC CONTROL DETAILS I- BR2	SHEET 19 OF 57
DESIGNED BY:	L.MAYNARD		





BRIDGE NO. 2, BRIDPORT
M. M. = 2.9

BRIDPORT DETOUR PLAN - BR2
NOT TO SCALE

BRIDGE
BR 5



PROJECT NAME:	BRIDPORT	PLLOT DATE:	8/22/2014
PROJECT NUMBER:	STP_CULV(29)	DRAWN BY:	J. SOTER
FILE NAME:	z11c2641col.br2.dgn	DESIGNED BY:	I. MAYNARD
PROJECT LEADER:	M. CHENETTE	CHECKED BY:	J. HUNGERFORD
DETOUR PLAN - BR2			SHEET 18 OF 57