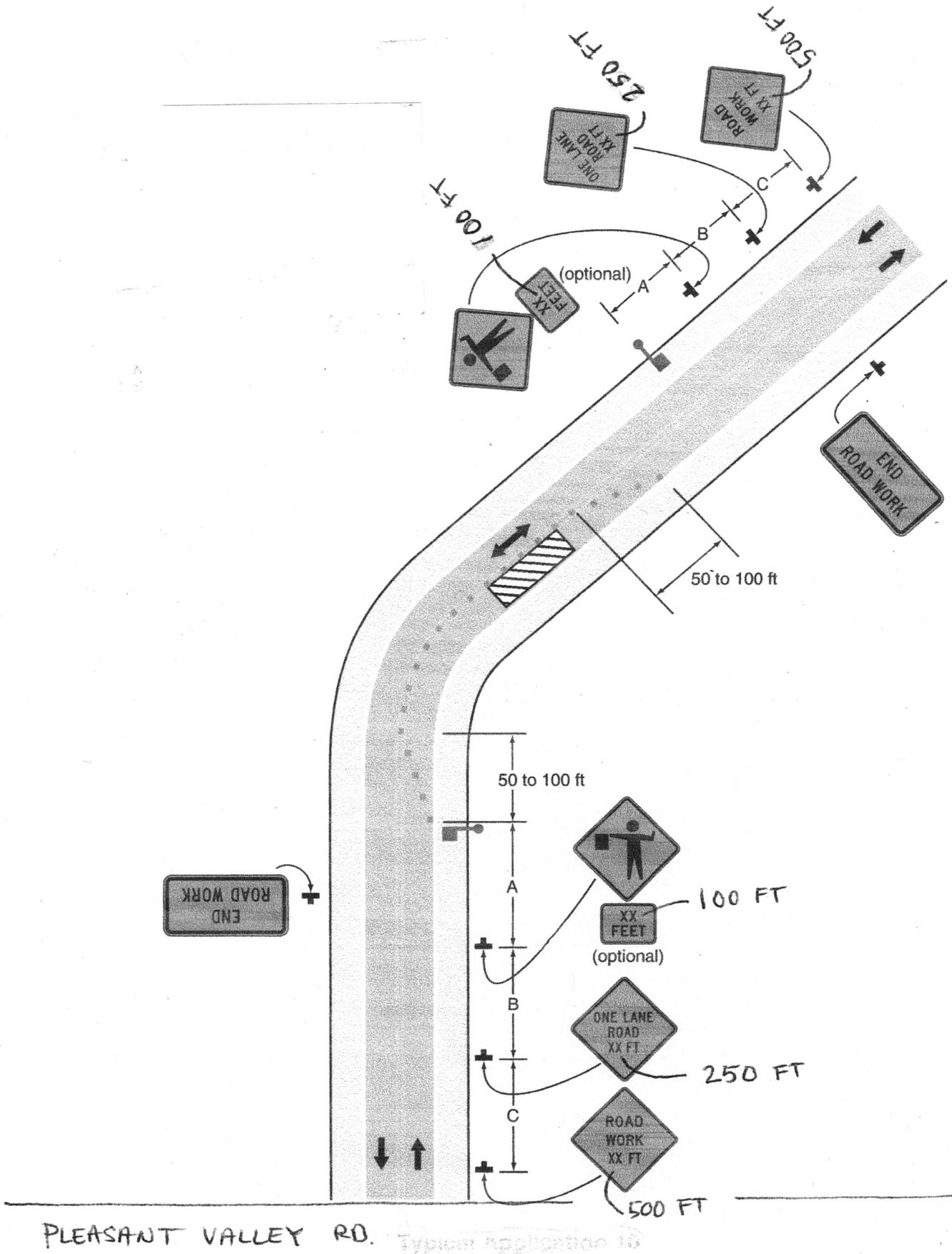


PRE AND POST BRIDGE CLOSURE PERIOD

PROJECT: CAMBRIDGE B20 1/4/8(39)  
TRAFFIC CONTROL PLAN

Figure 6H-10. Lane Closure on a Two-Lane Road Using Flaggers (TA-10)



**Notes for Figure 6H-10—Typical Application 10**  
**Lane Closure on a Two-Lane Road Using Flaggers**

**Option:**

1. For low-volume situations with short work zones on straight roadways where the flagger is visible to road users approaching from both directions, a single flagger, positioned to be visible to road users approaching from both directions, may be used (see Chapter 6E).
2. The ROAD WORK AHEAD and the END ROAD WORK signs may be omitted for short-duration operations.
- ~~3. Flashing warning lights and/or flags may be used to call attention to the advance warning signs. A BE PREPARED TO STOP sign may be added to the sign series.~~

**Guidance:**

4. *The buffer space should be extended so that the two-way traffic taper is placed before a horizontal (or crest vertical) curve to provide adequate sight distance for the flagger and a queue of stopped vehicles.*

**Standard:**

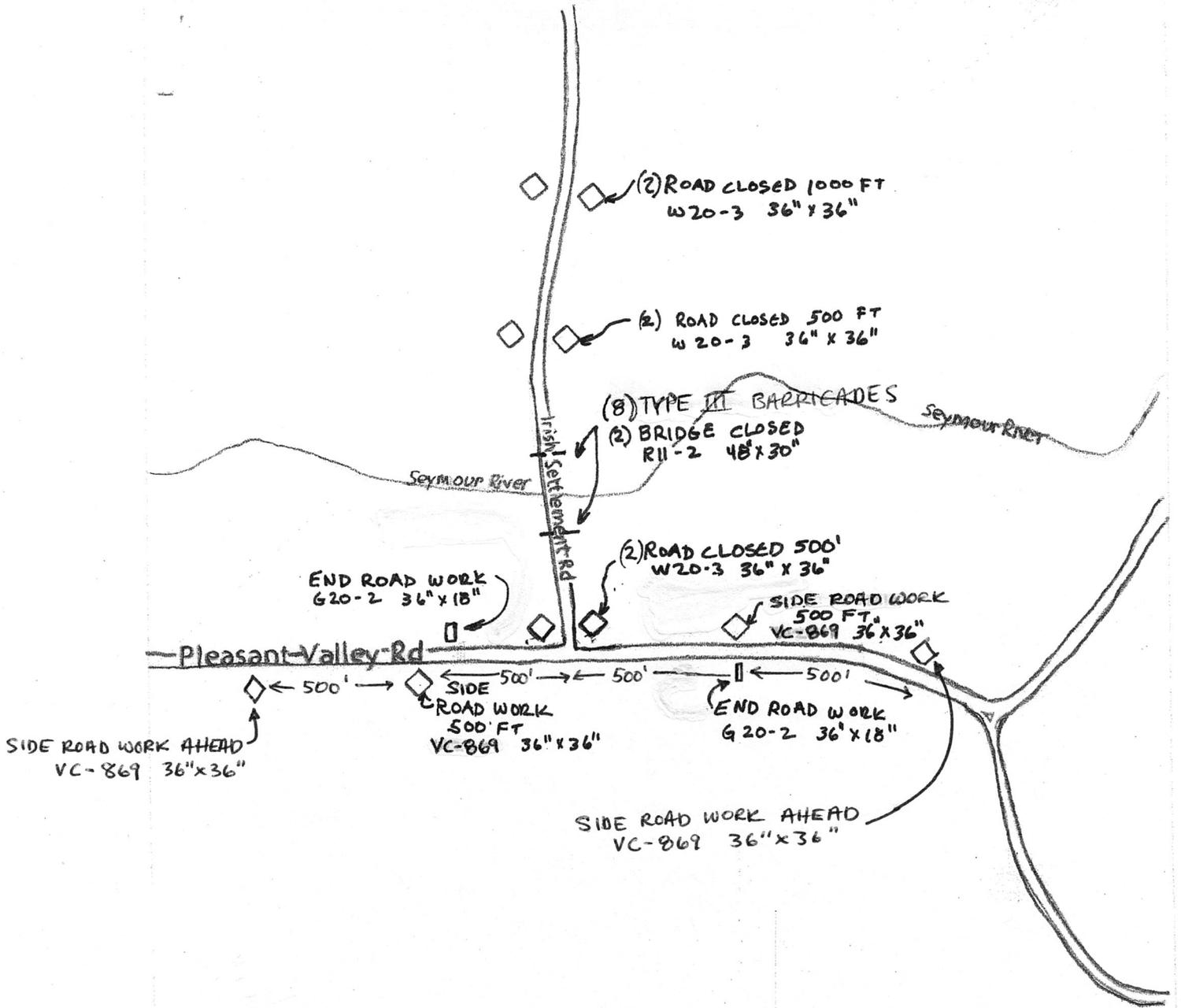
5. **At night, flagger stations shall be illuminated, except in emergencies.**

**Guidance:**

6. *When used, the BE PREPARED TO STOP sign should be located between the Flagger sign and the ONE LANE ROAD sign.*
7. *When a grade crossing exists within or upstream of the transition area and it is anticipated that queues resulting from the lane closure might extend through the grade crossing, the TTC zone should be extended so that the transition area precedes the grade crossing.*
8. *When a grade crossing equipped with active warning devices exists within the activity area, provisions should be made for keeping flaggers informed as to the activation status of these warning devices.*
9. *When a grade crossing exists within the activity area, drivers operating on the left-hand side of the normal center line should be provided with comparable warning devices as for drivers operating on the right-hand side of the normal center line.*
10. *Early coordination with the railroad company or light rail transit agency should occur before work starts.*

**Option:**

11. A flagger or a uniformed law enforcement officer may be used at the grade crossing to minimize the probability that vehicles are stopped within 15 feet of the grade crossing, measured from both sides of the outside rails.



STATE OF VERMONT  
 KRISTIN F. MCCOY  
 NO. 53906  
 CIVIL  
 LICENSED PROFESSIONAL ENGINEER

*Kristin McCoy*



PROJECT: CAMBRIDGE BR01448(39)  
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