



Weston Public Information Meeting

VT 100 – Bridge #98

Presented by



**Accelerated
Bridge
Program**
VTTRANS



Adamant Accord
Meeting Facilitation and Mediation Services



The Belden Company, Inc.
Rutland, Vermont



April 26, 2016

Introductions

Mark Mackintosh, P.E.

VTrans Southwest Regional Construction Engineer

Jennifer Fitch, P.E.

VTrans Project Manager

Scott Burbank, P.E.

VHB Project Manager

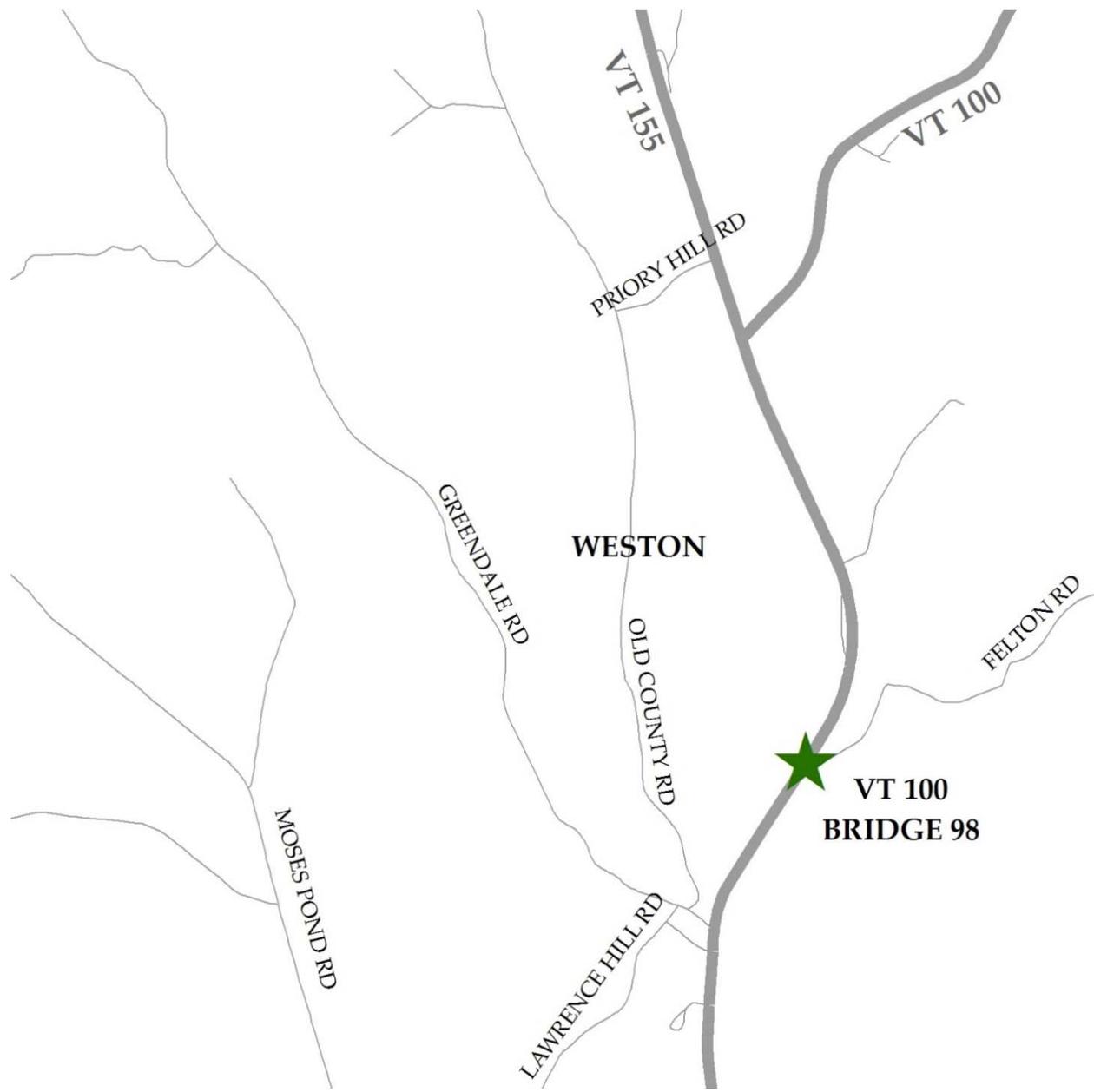
Ken Lougee

Belden Project Manager

Cindy Cook

Public Outreach Coordinator





Location Map

Meeting Overview

- Existing Bridge Conditions
- New Bridge Design
- Accelerated Bridge Program
- Bridge Closure
- How to Receive Project Updates
- Questions and Discussion





Existing Bridge

- Simple Span Steel Beam with Concrete Deck
- Reinforced Concrete Abutments
- Constructed in 1925



Existing Bridge

- Structurally Deficient
- Deck geometry is substandard
- Bridge railing is substandard
- Hydraulic capacity is substandard



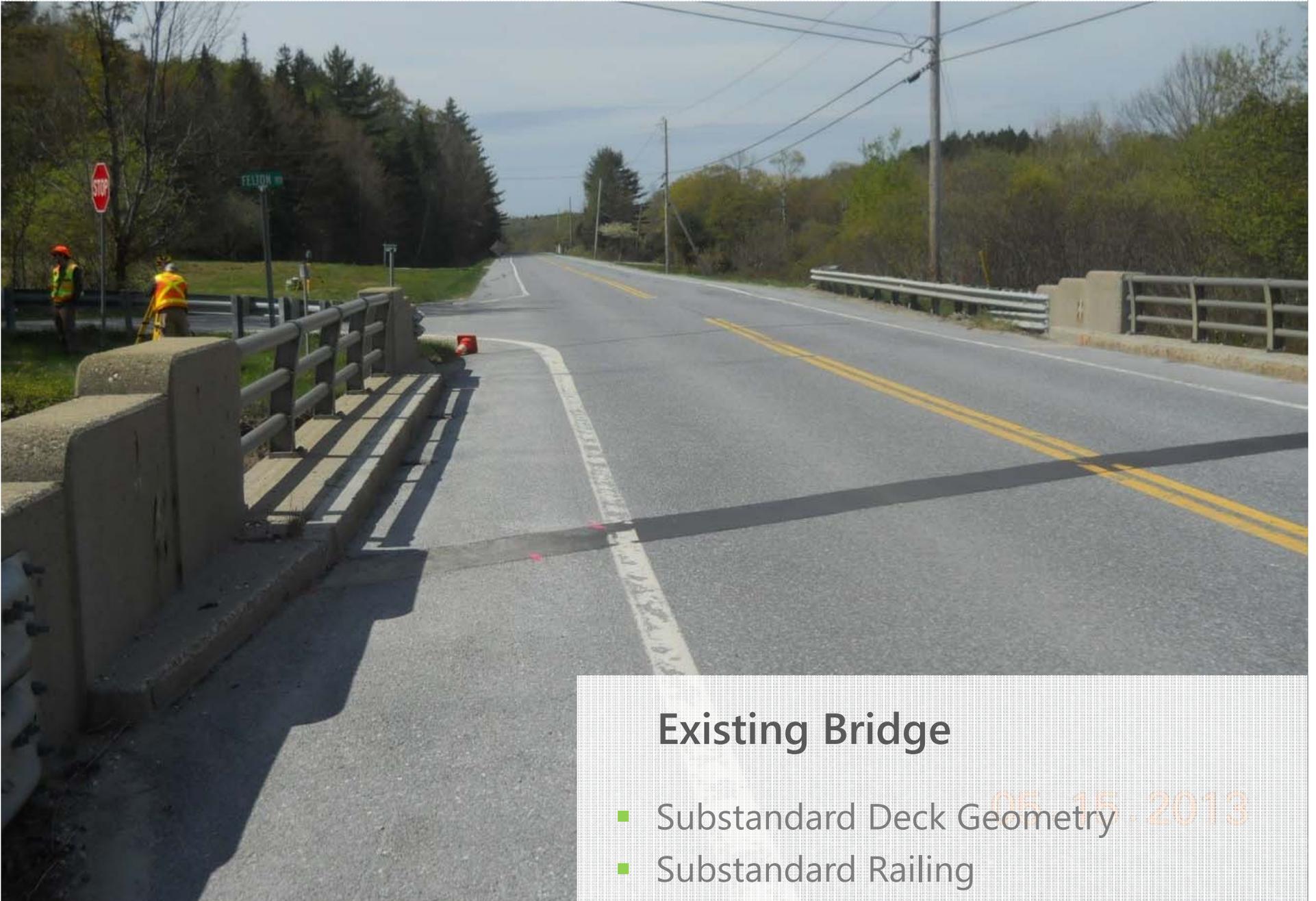
Existing Bridge

- Structurally Deficient
 - Substructure Rating 4 "Poor"



Abutment 1: Southeast corner

Approach looking south



Existing Bridge

- Substandard Deck Geometry
- Substandard Railing

05.15.2013

West Elevation



Existing Bridge

- Substandard Hydraulic Opening

Bridge Design

- Bridge and Roadway Widening
 - 11' Travel Lane and 5' Shoulder = 32' Roadway Width
(Existing: 12' Travel Lane and 3'-3" Shoulder = 30.5' Roadway Width)
- Increased Bridge Length and Slightly Raised Roadway to Provide Adequate Hydraulic Capacity
- Railing



Bridge Design

- Substructure: Precast Concrete Integral Abutment on Steel H-Piles



Bridge Design

Superstructure: Precast Prestressed Concrete
NEXT Beam Bridge



Construction Methods

- Accelerated Bridge Construction (ABC)
 - Rapid bridge construction techniques
 - Build bridge components off-site
 - Reduce bridge construction duration



Benefits of Accelerated Bridge Construction

- Reduced design and construction duration
- Reduced road user cost
- Safer for workers and traveling public
- Eliminated need for a temporary bridge
- Reduced impacts to:
 - Right-of-Way
 - Environmental Resources
 - Utilities



Construction Schedule

- ❖ Two week 24/7 alternating one-way traffic using temporary signals prior to bridge closure 5/17 – 6/2
 - ☐ Bridge will be open Memorial Day Weekend (Saturday through Monday)
- ❖ Bridge Closure 6/3 – 6/23
- ❖ Two week alternating one-way traffic during the day using flaggers after bridge closure 6/24 – 7/8
 - Two-way traffic during 4th of July

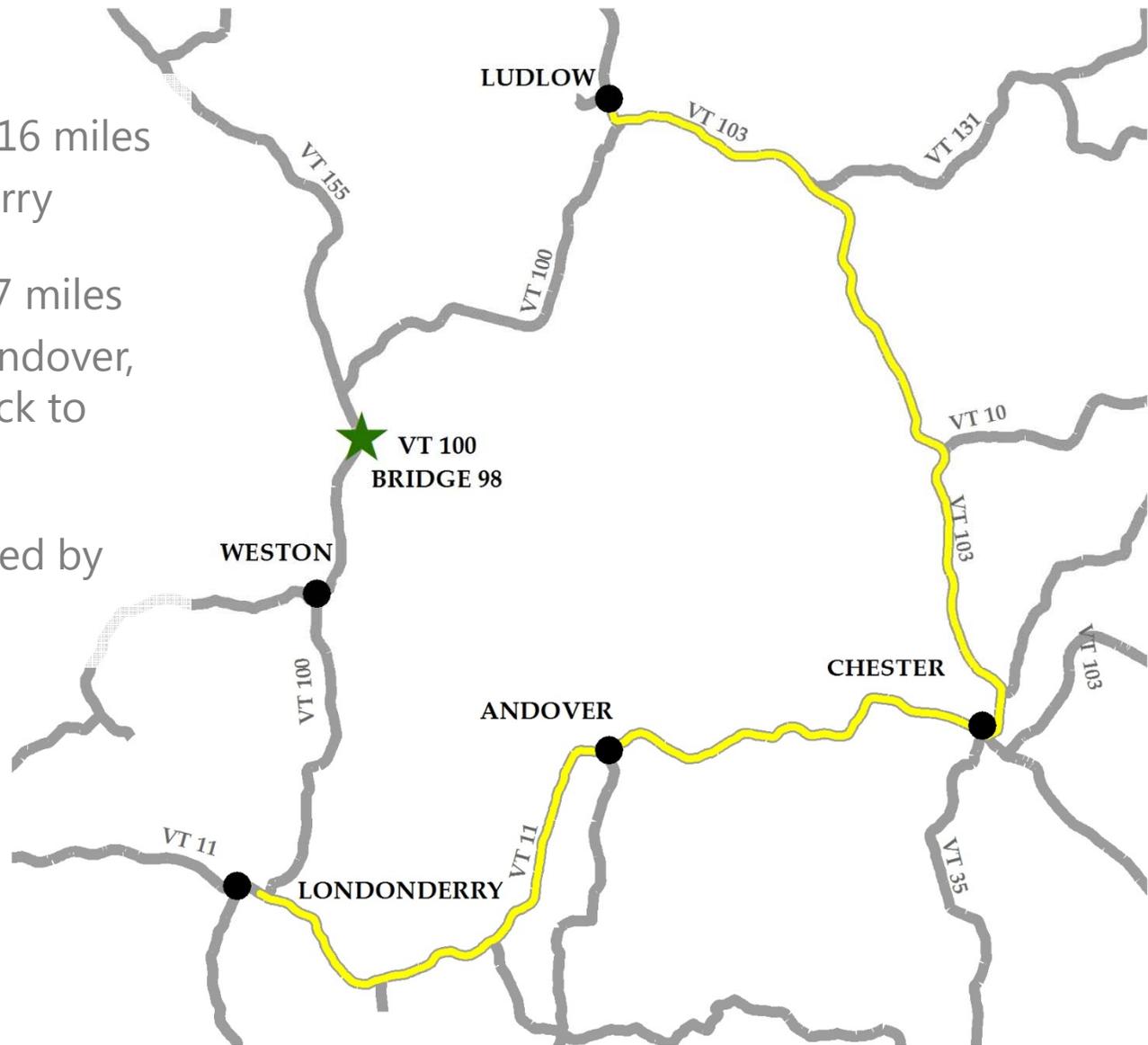
May '16						
S	M	T	W	T	F	S
1	2	3	4	5	6	7
8	9	10	11	12	13	14
15	16	17	18	19	20	21
22	23	24	25	26	27	28
29	30	31				

June '16						
S	M	T	W	T	F	S
			1	2	3	4
5	6	7	8	9	10	11
12	13	14	15	16	17	18
19	20	21	22	23	24	25
26	27	28	29	30		

July '16						
S	M	T	W	T	F	S
					1	2
3	4	5	6	7	8	9
10	11	12	13	14	15	16
17	18	19	20	21	22	23
24	25	26	27	28	29	30
31						

Regional Detour Map

- Through Distance = 16 miles
Ludlow to Londonderry
- Detour Distance = 27 miles
Ludlow to Chester, Andover,
Londonderry and back to
Weston
- Extra Mileage Required by
Detour = 11 miles



For More Information

If you'd like to receive weekly project updates, please contact:

Public Outreach Coordinator

Cindy Cook

Westonbridge@adamantaccord.com

802-917-5005



Questions?



Local Bypass



Questions?



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