



Randolph

TH 65 Bridge 35

Randolph Bridge 35 – BRO 1444(57)

PROJECT MILESTONES

Preliminary Plans

Fall 2013

Permitting

Winter 2014

Final Design

Spring 2014

Right-of-Way Complete

Summer 2014

Bid Advertisement

Late Summer 2014

Contract Award

Fall 2014

Target Construction Schedule

Summer 2015

Project Location: Town of Randolph in Orange County on Town Highway 65 (Palmer Road) at the Second Branch White River. The bridge is located approximately 500 feet west of the intersection with VT 14.

The Randolph TH 65 Bridge 35 project will replace the existing bridge, which has a sub-standard width and is in poor condition, with a new bridge on the existing alignment. The existing Randolph TH 65 Bridge 35 is a single span, one lane structure constructed in 1919. Palmer Road is a dead end gravel road with one homeowner at the end. The existing bridge is 28-feet in length and 13-feet wide. The existing superstructure is in very poor condition and currently has a temporary steel beam/wood deck structure spanning over it to allow for safe passage. The stream channel is constricted by the existing stone abutments, resulting in severe scour and undermining of the abutments.

VTrans evaluated alternatives for replacement of Randolph TH 65 Bridge 35 in an engineering study completed in April 2013. The study assessed the proposed design criteria for bridge and roadway alignment, right of way impacts, historical and archaeological resources, hydraulic and environmental resources. The evaluation resulted in a recommendation to replace the existing structure with a longer span bridge on the existing alignment. Two options for maintenance of traffic were considered. One option was to close the bridge to traffic during construction. The other option was to construct a new bridge downstream on a new alignment and maintain traffic on the existing bridge. The bridge closure option was chosen to reduce project costs and impacts to archaeological and historically sensitive areas.

The new bridge will be a Precast Prestressed Concrete NEXT Beam Bridge on precast concrete integral abutments which has a relatively short construction period and comparatively low construction and maintenance costs. The new bridge will be 55-feet in length and 19-feet wide including two 8-foot travel lanes and feature a galvanized 2 rail box beam with galvanized steel box beam type railing.

The bridge will be constructed with Accelerated Bridge Construction methods, which will expedite construction and reduce disturbance to the public. There will be an allowable 28 day road closure with temporary single lane closures prior to and following the bridge closure period.



East Abutment

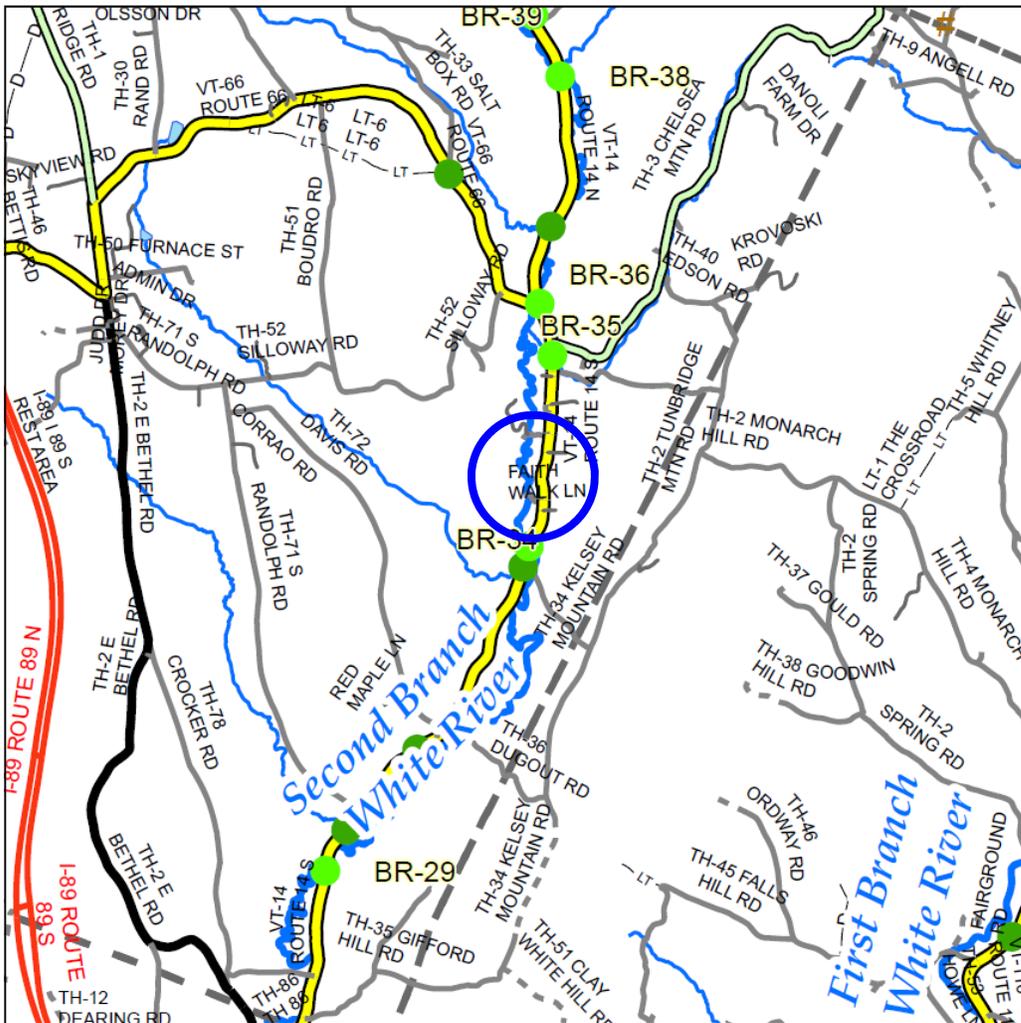


Target Construction Schedule: Construction activities are anticipated to take place beginning in May 2015. The allowable bridge closure period is one 28 day period between June 1, 2015 and September 1, 2015. Night work between the hours of 7:00 pm and 5:00 am will not be allowed. The contract is scheduled to be completed by September 25, 2015.

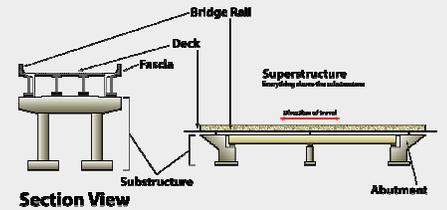
Contractor: The Contractor for Randolph TH 65 Bridge 35 is J. P Sicard, Inc.

Cost: Bids were opened on October 3 2014 and the low bidder was J. P Sicard, Inc. at \$602,426.

Project Manager: Todd Sumner, P.E.



Bridge Location Map



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