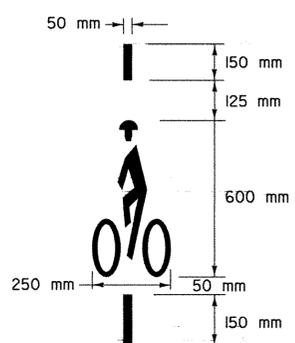


BICYCLE STENCIL PAVEMENT MARKING



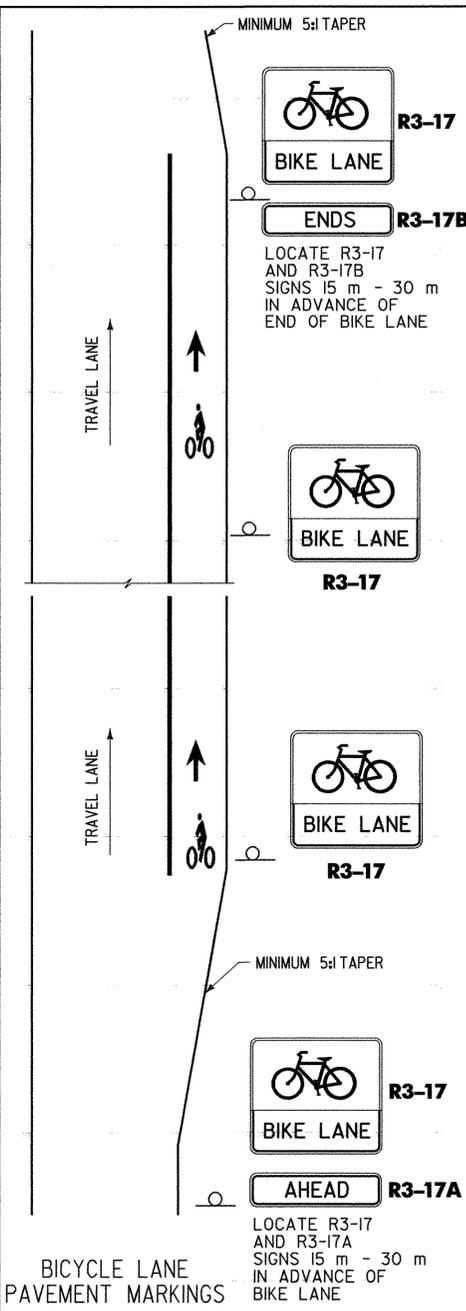
BICYCLE DETECTOR PAVEMENT MARKING

NOTE:
IF THE BICYCLE DETECTOR PAVEMENT MARKING IS USED, IT SHALL BE PLACED IN THE AREA OF HIGHEST SENSITIVITY OF VEHICLE DETECTOR LOOPS IN OUTSIDE TRAVEL LANES OR WHEN DETECTOR LOOPS ARE PLACED IN BICYCLE LANES. WHEN PAVEMENT MARKING IS USED, AN R10-22 SIGN SHALL BE INSTALLED.

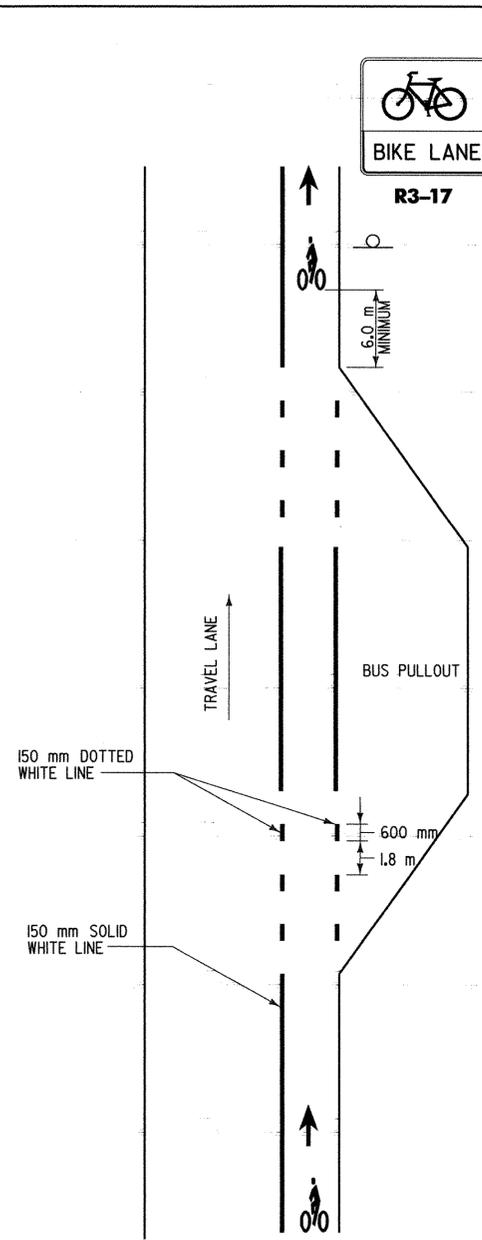
GENERAL NOTES:

- 1.) ALL BICYCLE LANE PAVEMENT MARKINGS TO BE WHITE RETROREFLECTORIZED PAINT OR MAXIMUM 2.25 mm THICK SLIP RESISTANT DURABLE MARKINGS.
- 2.) ADJUST LOCATION OF BICYCLE LANE PAVEMENT MARKING TO AVOID PLACEMENT WHERE IT IS LIKELY TO BE TRAVERSED BY VEHICLES, SUCH AS AT DRIVEWAYS.
- 3.) REFER TO VAOT PEDESTRIAN AND BICYCLE FACILITY PLANNING AND DESIGN MANUAL FOR GUIDANCE ON OTHER BIKE LANE CONFIGURATIONS.
- 4.) PLACE PAVEMENT MARKINGS AFTER STREET INTERSECTIONS AND AT INTERMEDIATE LOCATIONS ON LONG UNINTERRUPTED SECTIONS.

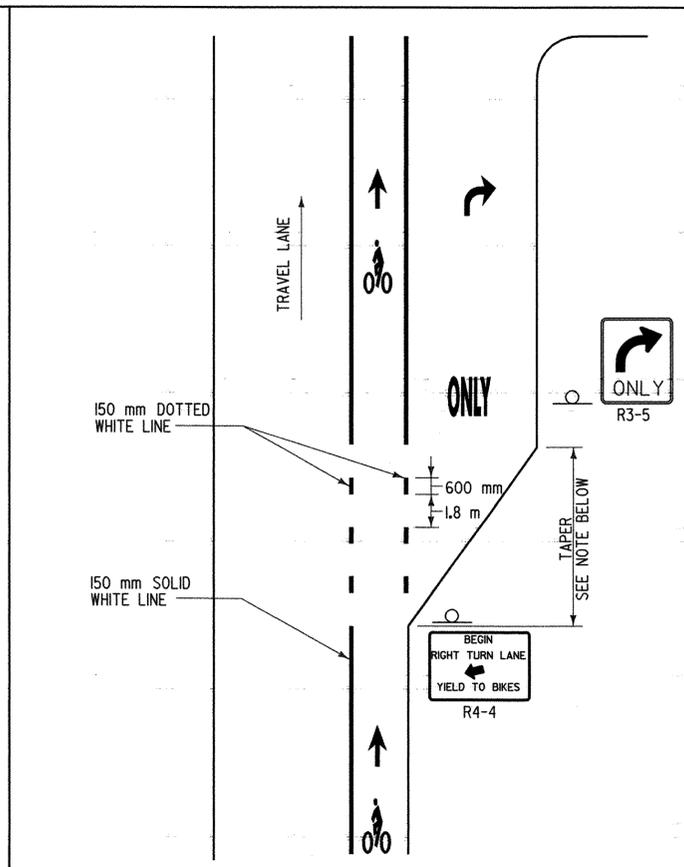
REVISIONS AND CORRECTIONS
APRIL 4, 2003 - DATE OF ORIGINAL ISSUE
MARCH 15, 2005 - REVISED SIGNS AND ADDED BICYCLE DETECTOR PAVEMENT MARKING



BICYCLE LANE PAVEMENT MARKINGS

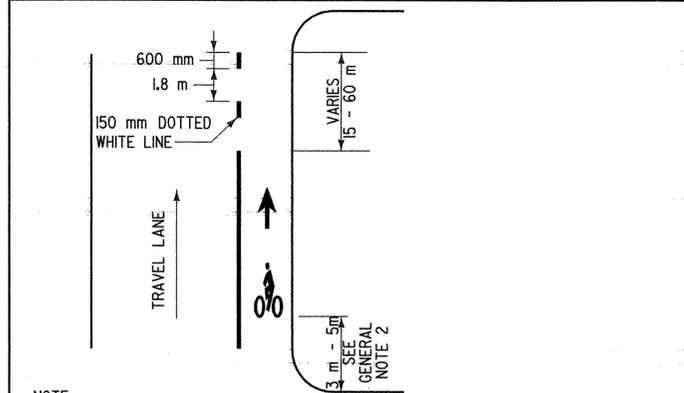


BUS PULLOUT WITH BICYCLE LANE PAVEMENT MARKINGS



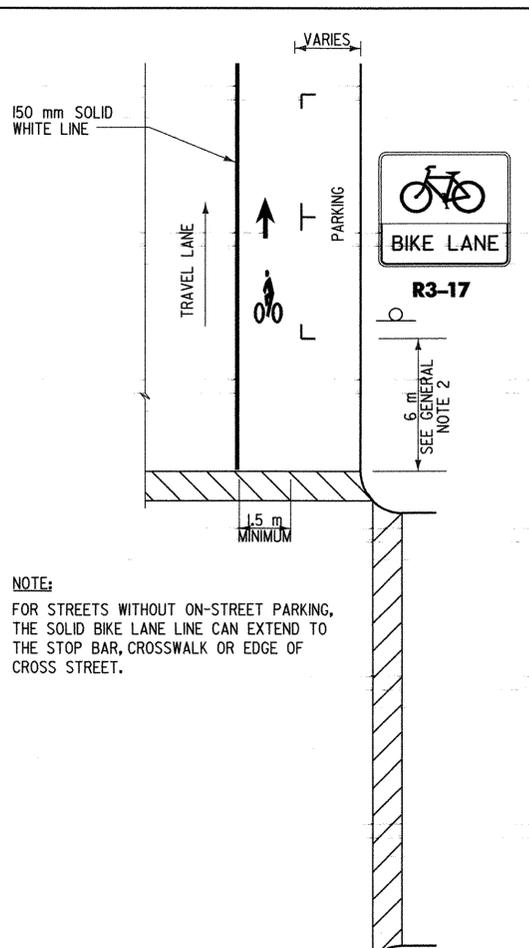
NOTES:
1.) REFER TO STANDARD DRAWING E-192M FOR DETAILS ON PAVEMENT MARKINGS FOR TURNING LANES.

BIKE LANE/RIGHT TURN LANE PAVEMENT MARKINGS

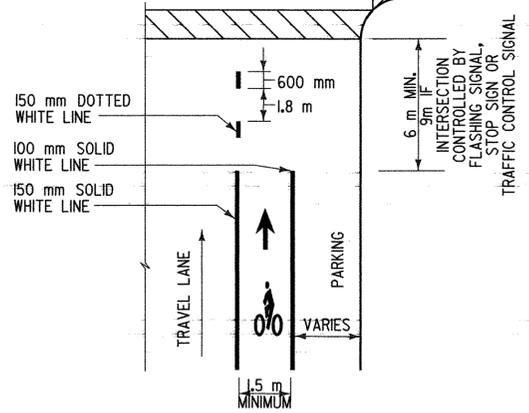


NOTE:
WHERE A NEAR SIDE BUS STOP OR HEAVY RIGHT TURN VOLUMES ARE PRESENT, A DOTTED LINE MAY BE USED. OTHERWISE MAINTAIN A SOLID LINE UP TO THE INTERSECTION, STOP BAR OR MARKED CROSSWALK

OPTIONAL BIKE LANE PAVEMENT MARKINGS WITH BUS STOPS OR HEAVY RIGHT TURN VOLUMES



NOTE:
FOR STREETS WITHOUT ON-STREET PARKING, THE SOLID BIKE LANE LINE CAN EXTEND TO THE STOP BAR, CROSSWALK OR EDGE OF CROSS STREET.



NOTE:
WHERE BICYCLE LANES ARE ADJACENT TO ON-STREET PARKING, A BICYCLE LANE WIDTH OF AT LEAST 1.5 m IS RECOMMENDED.

BIKE LANE PAVEMENT MARKINGS AT INTERSECTION WITH ON STREET PARKING

THIS SHEET IS NOT TO SCALE

OTHER STDS. E-143BM, E-172M, E-192M REQ'D:

APPROVED

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LOCAL TRANSPORTATION FACILITIES PROGRAM MANAGER

Michael J. ...
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BICYCLE PAVEMENT MARKINGS AND SIGN LAYOUT

