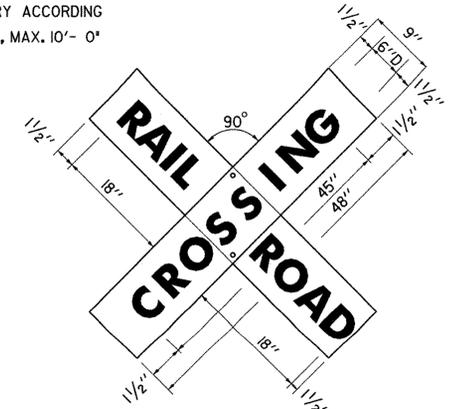


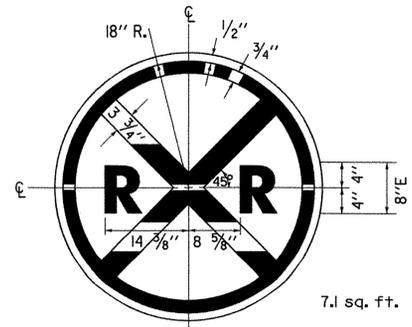
PAVEMENT MARKINGS

WIDTH MAY VARY ACCORDING TO LANE WIDTH, MAX. 10'-0"



RAILROAD CROSSING SIGN (CROSSBUCK)

R15-1



W10-1 ADVANCE SIGN

7.1 sq. ft.

SIGN MATERIALS AND COLORS

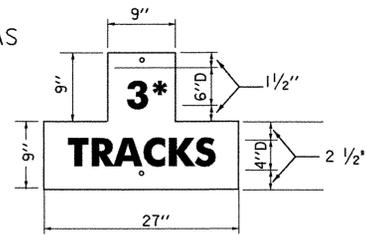
CROSSBUCKS SHALL BE 0.100" ALUMINUM WITH WHITE ASTM TYPE III OR TYPE V RETROREFLECTIVE SHEETING APPLIED TO BOTH SIDES OF SIGN PANELS

ADVANCE SIGN SHALL BE 0.125" ALUMINUM WITH YELLOW ASTM TYPE III OR TYPE V RETROREFLECTIVE SHEETING APPLIED TO THE ENTIRE BACKGROUND.

MULTITRACKS SIGN SHALL BE 0.060" ALUMINUM WITH WHITE ASTM TYPE III OR TYPE V RETROREFLECTIVE SHEETING APPLIED TO BOTH SIDES OF SIGN PANELS.

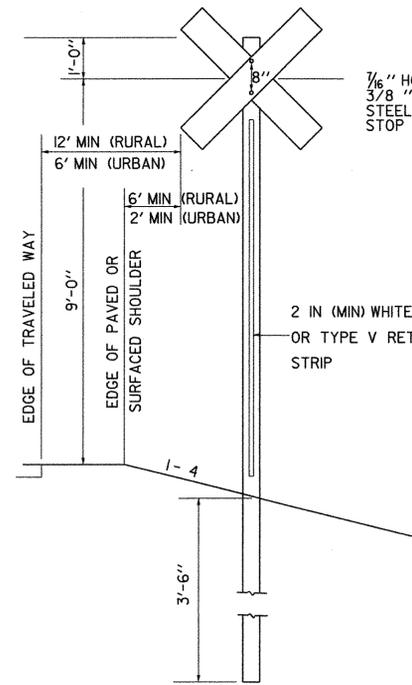
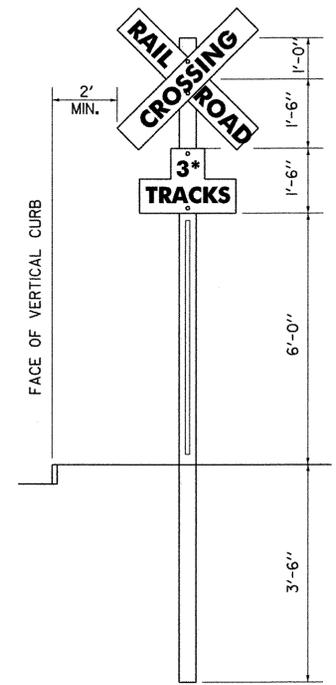
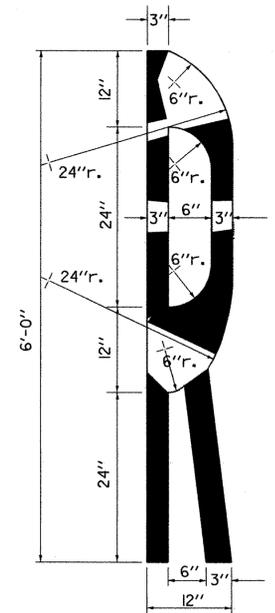
TEXT, STRIPES AND BORDERS SHALL BE EITHER SILK SCREENED OR LETTERING FILM.

* NUMBER AS REQUIRED



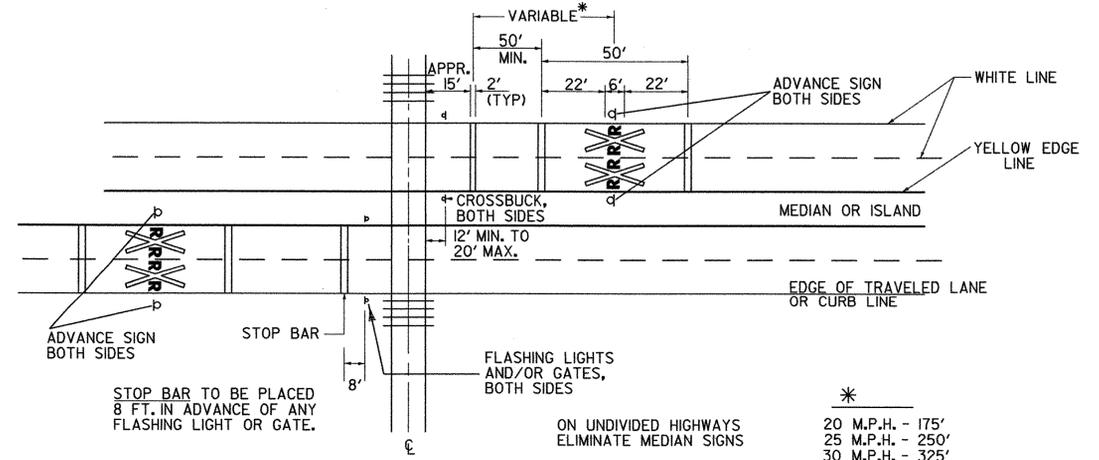
MULTI TRACKS SIGN

R15-2



7/16" HOLES FOR 3/8" x 5" STAINLESS STEEL BOLTS W/ELASTIC STOP NUTS

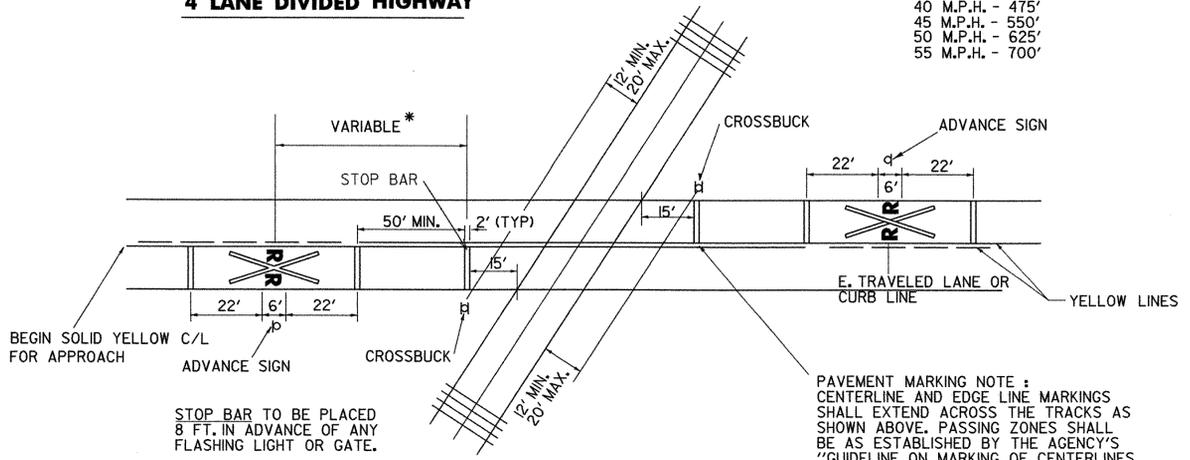
2 IN (MIN) WHITE ASTM TYPE III OR TYPE V RETROREFLECTIVE STRIP



4 LANE DIVIDED HIGHWAY

* ON UNDIVIDED HIGHWAYS ELIMINATE MEDIAN SIGNS

20 M.P.H. - 175'
25 M.P.H. - 250'
30 M.P.H. - 325'
35 M.P.H. - 400'
40 M.P.H. - 475'
45 M.P.H. - 550'
50 M.P.H. - 625'
55 M.P.H. - 700'



2 LANE HIGHWAY PAVED

ALSO APPLICABLE TO UNPAVED ROADS FOR PLACEMENT OF CROSSBUCKS AND ADVANCE SIGNS

PAVEMENT MARKING NOTE: CENTERLINE AND EDGE LINE MARKINGS SHALL EXTEND ACROSS THE TRACKS AS SHOWN ABOVE. PASSING ZONES SHALL BE AS ESTABLISHED BY THE AGENCY'S "GUIDELINE ON MARKING OF CENTERLINES ON TWO LANE, TWO WAY HIGHWAYS".

GENERAL NOTES

1. PAVEMENT MARKINGS

THESE MARKINGS SHALL BE PLACED, IF PHYSICALLY FEASIBLE, ON ALL PAVED APPROACHES TO RAILROAD CROSSINGS, WHERE THERE ARE SIGNALS AND/OR AUTOMATIC GATE AND AT ALL OTHER CROSSINGS WHERE THE PREVAILING SPEED OF HIGHWAY TRAFFIC IS 40 M.P.H. OR GREATER.

THE MARKINGS SHALL ALSO BE PLACED AT CROSSINGS WHERE ENGINEERING STUDIES INDICATE THERE IS A SIGNIFICANT POTENTIAL CONFLICT BETWEEN VEHICLES AND TRAINS. ALL MARKINGS SHALL BE WHITE EXCEPT FOR MEDIAN EDGELINES AND CENTERLINE ON 2 LANE ROADS WHICH SHALL BE YELLOW. THE RAILROAD CROSSING SYMBOL SHALL CONSIST OF THE "X" FLANKED BY TWO "R"s AND THREE 24" STOP BARS; TWO BRACKETING THE "X", ONE AT THE CROSSING. AT MINOR CROSSINGS OR IN URBAN AREAS, THESE MARKINGS MAY BE OMITTED IF AN ENGINEERING STUDY INDICATES THAT OTHER DEVICES INSTALLED PROVIDE SUITABLE PROTECTION.

2. CROSSBUCKS

WHERE PHYSICALLY FEASIBLE AND VISIBLE TO APPROACHING TRAFFIC, THESE SIGNS SHALL BE INSTALLED ON THE RIGHT-HAND SIDE OF THE ROADWAY FOR EACH APPROACH TO THE CROSSING. MULTI-LANE DIVIDED HIGHWAYS SHALL HAVE SIGNS MOUNTED ON BOTH SIDES OF THE ROADWAY APPROACHES.

3. AUXILIARY SIGNS

A "NUMBER OF TRACKS" SIGN SHALL BE USED WHERE THERE ARE TWO OR MORE TRACKS AT THE CROSSING.

4. ADVANCE WARNING SIGNS

RAILROAD ADVANCE WARNING SIGNS WILL BE PROVIDED AND LOCATED IN ACCORDANCE WITH SECTION 8B-3 OF MUTCD

5. SIGN POSTS

A STRIP OF ASTM TYPE III OR TYPE V RETROREFLECTIVE WHITE MATERIAL NOT LESS THAN 2 INCHES IN WIDTH, SHALL BE USED ON EACH SUPPORT AT HIGHWAY-RAIL GRADE CROSSINGS FOR THE FULL LENGTH OF THE FRONT AND BACK OF THE SUPPORT FROM THE CROSSBUCK SIGN OR NUMBER OF TRACKS SIGN TO NEAR GROUND LEVEL.

THIS SHEET IS NOT TO SCALE

OTHER STDS E-160 REQUIRED

REVISIONS AND CORRECTIONS
 SEPT. 10, 1987 - DATE OF ORIGINAL ISSUE
 JAN. 12, 1988 - REVISE LOCATION OF ADVANCE SIGNS
 DELETED ALUMINUM 3" X 3" TUBE
 JUNE 30, 2003 - ADDED RETROREFLECTIVE STRIP TO POST,
 CHANGED SHEETING TO ASTM TYPE III OR TYPE V
 RETROREFLECTIVE SHEETING

APPROVED
 DIRECTOR OF PROGRAM DEVELOPMENT
 TRAFFIC OPERATIONS ENGINEER
 FEDERAL HIGHWAY ADMINISTRATION

RAILROAD CROSSING SIGNS AND PAVEMENT MARKINGS



STANDARD E-190