

2023 VTrans Large-scale Bicycle and Pedestrian Grant Application

1. Project Title:

2. Applicant Name(s):

3. Project Contact Info:

a. Name:

b. Mailing Address:

c. Town:

d. Zip Code:

e. Email Address:

f. Phone Number:

4. Fiscal Information:

a. Accounting System Automated Manual Combination

b. Unique Entity Identifier #

c. Fiscal Year End Month

5. RPC(s)

6. Primary Facility Type: Sidewalk Bike Lane Shared-use Path

Shoulder

Other (Please describe)

7. Approximate project length in feet :

2023 VTrans Large-scale Bicycle and Pedestrian Grant Application

8. Project Description: Please give a brief description of the project (100 words or less.) Detailed information should be submitted as part of addressing the selection criteria. Be sure to include identifying streets or landmarks that the proposed project links at either end (e.g. New concrete sidewalk with granite curbing on Main St. from Elm St. to Maple St.).

2023 VTrans Large-scale Bicycle and Pedestrian Grant Application

9. Estimated Project Costs:

Engineering/Administration/Project Manager: Costs associated with survey, design, plans development, permitting, development of bid documents, bid analysis and Municipal Project Manager - typically around 25% of construction.

Engineering/
Admin/MPM Cost

Right of Way: Cost of appraisals, property owner compensation and associated legal fees (Minimum of \$5000 recommended).

ROW Cost

Construction: Cost of paying contractors to build projects, including a reasonable contingency. Please attach as much detail/ backup information as available to support the construction estimate.

Construction Cost

Construction Inspection : Cost to provide oversight of contractor during construction - typically around 15% of construction.

Const. Insp.Cost

TOTAL DESIGN/CONSTRUCTION AMOUNT APPLIED FOR :
(including 20% local share)

10. Have you received any other grant funding for this project? Please describe and include the source of funding:

2023 VTrans Large-scale Bicycle and Pedestrian Grant Application

11. Will you accept an award less than you applied for? YES NO

IF YES, please indicate below whether local funds will be used to make up the shortfall or if the project scope will be reduced:

Keep Scope of project the same and make up shortfall with other funds

Reduce project scope – Describe and provide cost breakdown (attach backup with supporting materials, if necessary)

Note: If the project scope is to be reduced, document what part of the project you would accept partial funding for and break out the costs associated with that part or segment. Attach additional pages if necessary. If adequate information is not provided, partial funding will not be considered. **Use Partial Funding Template provided by VTrans.**

2023 VTrans Bicycle/Pedestrian Program - Design/Construction Criteria Template

Applicant Name: Town of Warren

Project Title--Design/Construction: Sugarbush Access Road ADA Accessible Shared Use Path, Segment I (Inferno Rd to Golf Course Rd.)

Application Checklist – If any elements are missing, application may not be considered.

Make sure everything is included and pages numbered.

☒ (1) Project Application Form (separate PDF file)

All other materials noted below to be provided in the same order as below.

☒ (2) Project Evaluation Criteria Documentation for the project (completed BELOW)

☒ (3) Project Map(s)

☒ (4) Budget support information (e.g. detailed cost estimate)

☒ (5) RPC review confirmation letter

☒ (6) Current letter of support from the municipal governing body acknowledging their willingness to provide the local match and future maintenance responsibility

☒ (7) Documentation of contact with VTrans District office if project is on the state system

☒ (8) Supporting Documentation (scoping study or equivalent report, maps, and drawings) Note: If the scoping study is in a publicly accessible location online, applicants may provide a link with reference to relevant pages as appropriate.

DESIGN/CONSTRUCTION PROJECTS

1. **Community Need—25 Points:** How does the proposed project contribute to an existing or planned bicycle and/or pedestrian network? If the proposed project is a sidewalk along a street that already has a sidewalk, explain why the redundant facility is needed. What destinations or populations are served? What walking and/or bicycling access or safety problem are you trying to solve?

The purpose of Segment 1 of the ten-foot-wide ADA-Accessible Sugarbush Access Road Shared-Use Path (Path) is to provide a 0.7-mile-long (3,636 ft) off-road pedestrian connection from Inferno Road to Golf Course Road along the “Sugarbush Access Road Primary Section” alignment of the Town of Warren Sugarbush Access Road Path Scoping Study (Warren STP BP15(13)). The western terminus of Segment 1 is Sugarbush Resort (at Inferno Road), with the eastern terminus being Workforce Housing/Lower Woods Trail Network Trailhead/proposed Mad Bus bus stop structure (at Golf Course Road). This Path will:

1. Provide a critical pedestrian connection for 250 seasonal and year-round employees of Sugarbush Resort, residing in workforce housing along the Path’s alignment;
2. Establish safe access points for users of the regional public transit system, Green Mountain Transit’s Mad Bus service;
3. Create an off-road connection for users of the existing and popular 3.6-mile-long Inferno Road - West Hill Road - Golf Course Road recreational loop;
4. Develop an off-road connection to the Lower Woods Trail Network;
5. Address existing pedestrian safety challenges of the residents of 460 residential units, 9 single family homes, 5 businesses, and Sugarbush Resort along the Path’s alignment.
6. Establish the first piece of critical pedestrian connection for the 45+/- residents of Downstreet Housing & Community Development’s Wheeler Brook Apartments;
7. Establish the first piece of critical pedestrian connection for four condominium complexes, one inn, three businesses and seven single family homes located on the Access Road between Golf Course Road and Wheeler Brook Apartments;

(1) Approximately 80% of Sugarbush Resort employees who live and work at the resort (in particular, workers who arrive from abroad on seasonal work visas) do not have access to private transportation. As a result, they regularly travel to/from work via bus or walk on the busy, unsafe, and often snow-covered shoulder of the Sugarbush Access Road (Access Road). During the 2022-23 ski season, this was the experience of 38 workers housed across three Sugarbush workforce housing buildings. By the 2025-26 ski season, Sugarbush anticipates housing up to 250 employees along Segment 1. This will be made possible through the construction of two additional workforce housing (WFH) complexes currently under development by Sugarbush Resort:

- Sugar Cubes (2197 Sugarbush Access Road): 12 bedrooms across 3 dwellings, housing up to 24 employees. This project received local permits in May 2023, commencing construction in Fall 2023, and has an estimated completion date of late 2024.
- Sugarbush WFH at Rosita’s (2500 Sugarbush Access Road): 180 single occupancy bedrooms and 20 studio apartments, housing a total of 200 employees. This project is currently under review by the Warren DRB, with construction commencing in spring 2024 and anticipated completion in 2025.

While Sugarbush Resort is making major improvements in the quality of life of its workers by creating comfortable, modern, workforce housing, we believe that a safe path to and from work is an essential service.

(2) The regional public transit system, Green Mountain Transit's (GMT) Mad Bus, which provided 36,721 rides during the 2022-23 ski season, is planning designated stops at each of the proposed WFH complexes as well as the Lower Woods Trailhead (including a bus shelter at Segment 1's eastern terminus of the Lower Woods Trailhead/Golf Course Road). In addition, the Mad Bus makes "on demand" stops along the Access Road. Currently, bus users must walk along the shoulder to access the bus. The proposed Path will provide safer access to public transit.

(3) The 3.6-mile-long Inferno Road - West Hill Road - Golf Course Road - Access Road loop is an existing popular walking, biking, and running route used regularly by residents and visitors alike. Most of this route is on secondary roads, with the exception of the heavily traveled segment along the Access Road. The proposed Path will take users off the heavily traveled Access Road, increasing safety, and ensuring the entirety of the loop is on secondary roads and this off-road path.

(4) The 1.5-mile-long Lower Woods Trail Network provides off-road access to German Flats Road and the broader MRV trail system, bringing us a step closer to an off-road connection to downtown Waitsfield.

(5) Sugarbush Resort's Lincoln Peak Village and its concentrated higher development densities and commercial activity is focused on growing 4-season activities. Sugarbush currently sees greater than 500 thousand individual visitors each calendar year. As such, it is an obvious bicycle/pedestrian generator in an area that has historically had inadequate or non-existent bicycle and pedestrian accommodations.

(6) Downstreet Housing & Community Development's Wheeler Brook Apartments, built in 2009, provides homes to 45+/- residents, including single parents and children. Sugarbush Resort is working to create affordable and accessible recreational opportunities for the residents of Wheeler Brook. While Segment I will stop short of Wheeler Brook, it is a crucial step in meeting the goal set out in the Scoping Study of connecting Wheeler Brook with the resort and the broader active transportation network - thus enhancing the quality of life of its residents. Segments II and III of the project will complete this connection in following years.

Segment 1 of the Sugarbush Access Road Path will implement a key "Major Off Road Connection" identified in the Mad River Valley Moves Active Transportation Plan's "Future Opportunities Map," funded in the pilot round of the VTrans Better Connections Program (2015).

The project also addresses concerns and aspirations of the 2019 Warren Town Plan:

- Frequent unsafe pedestrian activity along the shoulders and the edges of the pavement of Sugarbush Access Road due to the lack of definition as to where pedestrians should walk, especially at the western end;
- The existing one to two-foot-wide paved shoulders on Sugarbush Access Road create uncomfortable conditions for walkers, especially in the winter when snow can limit their ability to move off the paved portion of the road as trucks or vehicles pass nearby in the travel lane;

- The use of Sugarbush Access Road by over 3,900 vehicles daily during the busy winter months for Sugarbush Resort;
- The safety concerns of the Town prompted a request for a VTrans safety study of Sugarbush Access Road, which resulted in numerous recommendations to improve safety for motorists, but not for pedestrians or bicyclists;
- Current Green Mountain Transit bus routes, The Mad Bus, and with stops along Sugarbush Access Road, do not have adequate pedestrian access between the stops nor bus rider origins or destinations;

Pedestrian infrastructure in this high-profile, densely populated location will further community-wide goals of establishing *a safe, environmentally friendly, and integrated transportation network*, both increasing the desirability and feasibility of non-vehicular travel (Mad River Valley Vision Statement, 2015).

2. Economic Development—10 Points: How does the project contribute to broad local community and economic development goals? How does the project contribute to ongoing local placemaking or economic development initiatives?

The Sugarbush Lincoln Peak Village is the focus of the Town of Warren's economic activity and development, containing the bulk of Warren's recent commercial and residential development. As such, this area is designated as the town's Primary Growth Center in the 2019 Warren Town Plan (pg. 74). The proposed shared-use path is located in the heart of this Primary Growth Center and along its principal transportation corridor. The proposed project fulfills the Town Plan's goals of developing the area...*in a manner that reflects the ski village scale and density...and pedestrian orientation of development* (pg. 78). The project also furthers the Town Plan Goal to *support the economic viability of Sugarbush Resort and other businesses that contribute to the Valley's attractiveness as a four-season resort destination* (#2, pg. 2).

In addition, the Mad River Valley Moves Active Transportation Plan (2016) specifically identifies this multi-use path as a "Major Off Road Connection" in its "Future Opportunities Map" due to its ability to serve as a critical travel corridor connecting the major destination of Sugarbush Resort's Lincoln Peak Village with surrounding residential and commercial establishments. Specifically, the MRV Moves Active Transportation Plan recommends "Major Off-Road Connections" to be physically separated from motor vehicle traffic and provide accessible outdoor recreation and transportation opportunities for all user groups, including adult bicyclists, child bicyclists, horseback riders, walkers, joggers/runners, wheelchair users, hand cyclists, baby strollers, dog walkers, snowshoers, and cross country skiers. The proposed Path will be ADA accessible and specifically accommodates user groups with unmet needs identified in the MRV Moves Active Transportation Plan.

The Mad River Valley Economic Study (2014) identified the recreation and tourism economy to be one of four major drivers of the MRV economy and characterized it as evolving from skiing dominance to more year-round activity. Specifically, it identified connectivity of recreation assets to be a barrier to economic vibrancy, for both the existing and evolving economic landscape, and recommended expansion of such access and connectivity as a major next step. The 2014 Mad River Valley Economic Study recommends creating more accessible, point-to-point, publicly-accessible off-road transportation infrastructure – especially those facilities that might

enhance tourism objectives. This recommendation led to the development of the MRV Moves Active Transportation Plan (2016), which specifically identifies the need for the proposed project - a shared-use path along Sugarbush Access Road.

The improved quality of life and working conditions achieved by a combination of new workforce housing construction by Sugarbush Resort and safe pedestrian access to and from work will in turn make employment at Sugarbush Resort more desirable, and, over time, attract a higher quality workforce, which will improve the guest experience and increase economic activity.

The improved access to Green Mountain Transit will increase ridership.

The safer recreational loop and access to MRV trails will improve guest and resident experiences, making the area more attractive to residents and visitors, increasing tourism in the MRV.

The eventual connection between Wheeler Brook Apartments and Sugarbush Resort will create outstanding recreational opportunities for its residents, improving the health and wellness of a vulnerable population and increasing the resident's success at their places of work and school.

- 3. Well-supported budget —20 points:** How were the project costs developed? Are all required project elements (admin, engineering, construction, inspection) adequately budgeted for? Be sure to include backup documentation for project costs. Include reasonable contingency for inflation over the life of the project.

The June 2023 Project Budget was developed utilizing the cost estimate included in the Sugarbush Access Road Path Scoping Study [Warren STP BP15(13)], published 2/23/17, as a foundation. This 2017 Study was authored by Broadreach Planning & Design with engineering design and cost estimates provided by Lamoureux & Dickinson Consulting Engineers.

The Sugarbush Access Road Path Scoping Study breaks the entirety of the path project into four color phases. This grant application seeks construction of the entirety of the Orange Phase (Inferno Road. to Paradise Deli) and approximately 1/3 of the Blue Phase (Paradise Deli to Golf Course Road. - 775' in length). The bounds of this application is Inferno Road. to Golf Course Road., identified as Segment 1. The project estimate includes the entirety of Segment 1, with cost detail (unit/quantity/unit cost/cost) broken down by the Scoping Study's applicable color phases (Orange & 775' of Blue).

The "Initial Estimate of Probable Construction Costs" for each of these phases can be found on pg. 27-30 of the Scoping Study, and serve as the basis of the project budget submitted in this application. In addition to construction costs, the cost estimate includes Contingency (@15%), Engineering (@15%), ROW (@1%), Project Management (@9.5%), and Construction Oversight (@15%).

The attached project budget submitted with this application has evolved since the 2017 Scoping Study's cost estimate - it uses *VTrans 2-Year Weighted Bid Averages (2021-2023)* and

references the *VTrans January 2020 Report on Multi-Use Path and Sidewalk Costs* to reflect current unit costs and ensure all elements were adequately budgeted for. This is the third time the 2017 Study's estimate has been updated (2019 & 2022 Bike & Ped Program applications), each time refined in consultation with Dough Henson, who served as project principle on the 2017 Scoping Study while Vice-President of Lamoureux & Dickinson, and now Survey Project Manager with Trudell Consulting Engineers. Joshua Schwartz, Executive Director of the Mad River Valley Planning District, who has been intimately involved with this project since its origins in 2015, took the lead in completing the 2023 budget. Both Doug & Joshua hold nuanced knowledge of the proposed project, and Doug's professional insight of cost changes and necessary contingency. Additional guidance and consultation was provided by Pat Travers, Project Manager at GPI / Greenman-Pedersen, Inc.

- 4. Complexity—10 points:** What complexities does your proposed project have and how do you plan to address them? Response must address need for right of way, anticipated permitting, natural resource constraints or identified cultural resource (historic or archaeological) impacts anticipated for the project. If a scoping or planning report is attached, please highlight or reference the applicable sections.

As laid out in the Sugarbush Access Road Path Scoping Study's "Permits, Easements, and Approvals" section (pg. 30), the proposed Segment I, from Inferno Road to Golf Course Road, doesn't include complexities in regards to right-of-way (ROW) or cultural resources. The Scoping Study identifies a Path alignment located mostly within the ROW, with the exception along the existing gravel Sugarbush Resort Parking Lot Path on the far western end of the route. This section is on Sugarbush Resort property and remains outside of the town ROW. Amendment to Sugarbush Resort's existing Conditional Use permit may be needed for the enhancement of the existing gravel Sugarbush Resort Parking Lot Path to meet ADA requirements for slope, cross slope, and surface. This is the only local permit amendment anticipated and has been discussed with both Sugarbush Resort and the Warren Zoning Administrator. As detailed in their attached Letter of Support, Sugarbush Resort is excited to participate in this public-private partnership and help realize a successful community-driven project. Sugarbush Resort has committed to facilitating amending the Conditional Use and state permits as needed, as well as granting a permanent right-of-way for the path, granting temporary slope and construction easements and allowing project construction staging to occur on resort lands.

As identified in the Scoping Study, the project itself will not trigger Act 250. However, Sugarbush may need to amend its Act 250 permit regarding the Path on its property if required by the State. As laid out in their Letter of Support, Sugarbush is committed to securing such necessary approval from the Natural Resources Board.

The "Permits, Easements, and Approvals" section (pg. 30) also identifies a possible need for a Stormwater Operational Permit as the project may exceed the threshold for new impervious surface. The project team will work with the State of Vermont Stormwater Division to secure this permit if required.

The eastern terminus of the path alignment falls within the buffer of a wetland identified on the north side of Sugarbush Access Road across from Golf Course Road (on Sugarbush Resort-owned property). The need for a wetland permit is not anticipated but close coordination with the State of Vermont Wetlands program will occur.

The Sugarbush Access Road Bicycle & Pedestrian Scoping Study's "Preferred Alignment Issues" map ("Page 40, Figure 3," included in this application) identifies a multitude of project design considerations, including:

- Sugarbush Access Road/Inferno Road intersection modifications to better accommodate bicyclists and pedestrians;
- Crossing over Rice Brook via a new prefabricated bridge (100' x 11');
- Relocation of a utility pole, narrowing of the path between an existing retaining wall and road
- Construction of a new retaining wall (100' x 3'), etc.

All of these design constraints have been explored through the Scoping Study process and the professionally developed plans for addressing them are detailed in the document. The resources necessary to address them have also been incorporated into the project budget.

- 5. Project coordination – 5 points:** To your knowledge, are there other state or local projects in the same area that might impact the project timeline and schedule for completion? Is the project on a state-maintained route? Is the funding being used for elements of a larger project funded through other sources?

There are no conflicting projects that might impact the project timeline and schedule for completion. The project route is not along a state highway; Sugarbush Access Road is a municipal road. Sugarbush Access Road received full-depth reclamation paving in 2014, and a full shim-paving in May 2022. The requested funding would be used solely for this project, and the local match has been fully committed and set aside.

- 6. Equity—10 Points:** How does your project directly address the needs of more vulnerable populations, specifically the needs of children, older persons, people with mobility challenges and low- or moderate-income households? What outreach was performed to include disadvantaged communities, especially low income, BIPOC, people with disabilities and others, in the planning of this project.

The proposed Path, Segment I will:

1. Provide a critical pedestrian connection for 250 seasonal and year-round employees of Sugarbush Resort residing in workforce housing along the proposed path (all WFH

complex construction to be completed by 2025). Up to 80% of these workers do not have access to personal transportation to and from the resort.

2. Establish safe access points for the users of Green Mountain Transit's Mad Bus system.
3. Bring the critical pedestrian connection a step closer to the residents of Downstreet Housing & Community Development's Wheeler Brook Apartments.
4. Provide a safe ADA accessible Path for all pedestrian and recreational users, including adult bicyclists, child bicyclists, horseback riders, walkers, joggers/runners, wheelchair users, hand cyclists, baby strollers, dog walkers, snowshoers, and cross-country skiers.

7. **Multi-modal potential —5 points:** How does your proposed project coordinate with other modes of transportation? Will it improve walking or bicycling access to transit, rail service or park and ride facilities?

The proposed Segment I of the Sugarbush Access Road Path will provide a direct connection to

1. Green Mountain Transit's Mad Bus: Valley Floor (Route 120), Mt. Ellen Shuttle (Route 122), & Sugarbush Access Road (Route 125). During the 2022-23 season, these routes saw 8,150, 13,212, and 9,857 riders, respectively.
2. Existing pedestrian/bicycle infrastructure at Sugarbush's Lincoln Peak Village (see attached Map 3 (*Sugarbush Access Road Shared Use Path - Preferred Segment I Alignment with Adjacent Parcel Owners*))
3. Lower Woods Trail Network with connection to the broader MRV system of multi-use trails
4. A multitude of pedestrian-scale housing developments adjacent to the route (four workforce housing facilities, ten single family homes/lots, The Bridges Resort, Sugar Lodge, and the Maples Condominiums).

Segment I of this project has independent utility at its eastern end terminates at the Sugar Cube workforce housing complex, the Lower Woods Trailhead, a designated Green Mountain Transit Mad Bus stop (with a proposed bus stop structure), and the Access Road's intersection with Golf Course Road. Its western terminus ends at the Inferno Road. four corners, with a direct connection to Sugarbush Resort Lincoln Peak Village, a sidewalk network, and dense housing development. Segment II will extend the Path to Eurich Pond Road, Segment III to Downstreet Housing & Community Development's Wheeler Brook Apartments, and Segment IV to the Mad River Path connection at the junction with Rt. 100.

8. **State designated centers —5 points:** Is the proposed project within a state designated center?

The proposed project is not within a state-designated center. However, it is located entirely within the town's Primary Growth Center as identified in its 2019 Warren Town Plan.

9. **Project Management—10 Points:** Describe your plan for keeping this project moving forward. What management practices do you now have, or plan to put in place, to

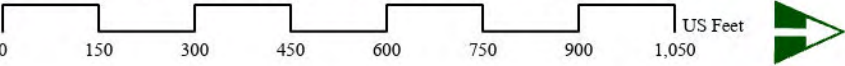
successfully administer the project from design through construction? Who will manage the project (municipal staff, RPC, consultant, or other)?

The Sugarbush Access Road Shared Use Path is a top priority for the Town of Warren, Sugarbush Resort, Mad River Valley Planning District, Mad River Path Association, Mad River Riders, and the MRV Chamber of Commerce. The Sugarbush Access Road Path Scoping Study (Warren STP BP15(13) brought together attention, participation, problem-solving, and momentum focused on implementing this “Major Off-Road Connection.” The history goes back as far as 1987’s Preliminary Feasibility Study – Sugarbush Access Road Pathway Design. All of these partners, plus property owners along the Sugarbush Access Road, are ready to see this long-awaited project come to reality. The Town of Warren is well positioned and has the necessary resources to manage the project, as well as the direct staff support and resources of the Mad River Valley Planning District and Mad River Path Association. As it has done successfully many times before, the Town of Warren will create a local project Steering Committee (including representatives from the Town of Warren, Mad River Valley Planning District, Mad River Path Association and Sugarbush Resort) with the direct purpose of ensuring project success. This will include initial and ongoing project communication, procurement of a consultant to serve as the Local Project Manager, and support for the Local Project Manager through the duration of the project. All grant-related and other financials will be handled by Warren’s Town Administrator, who has a strong record of successfully administering complicated federal and state projects (including the Warren Main Street Reconstruction Project, numerous paving, culvert, and bridge projects, and many more).

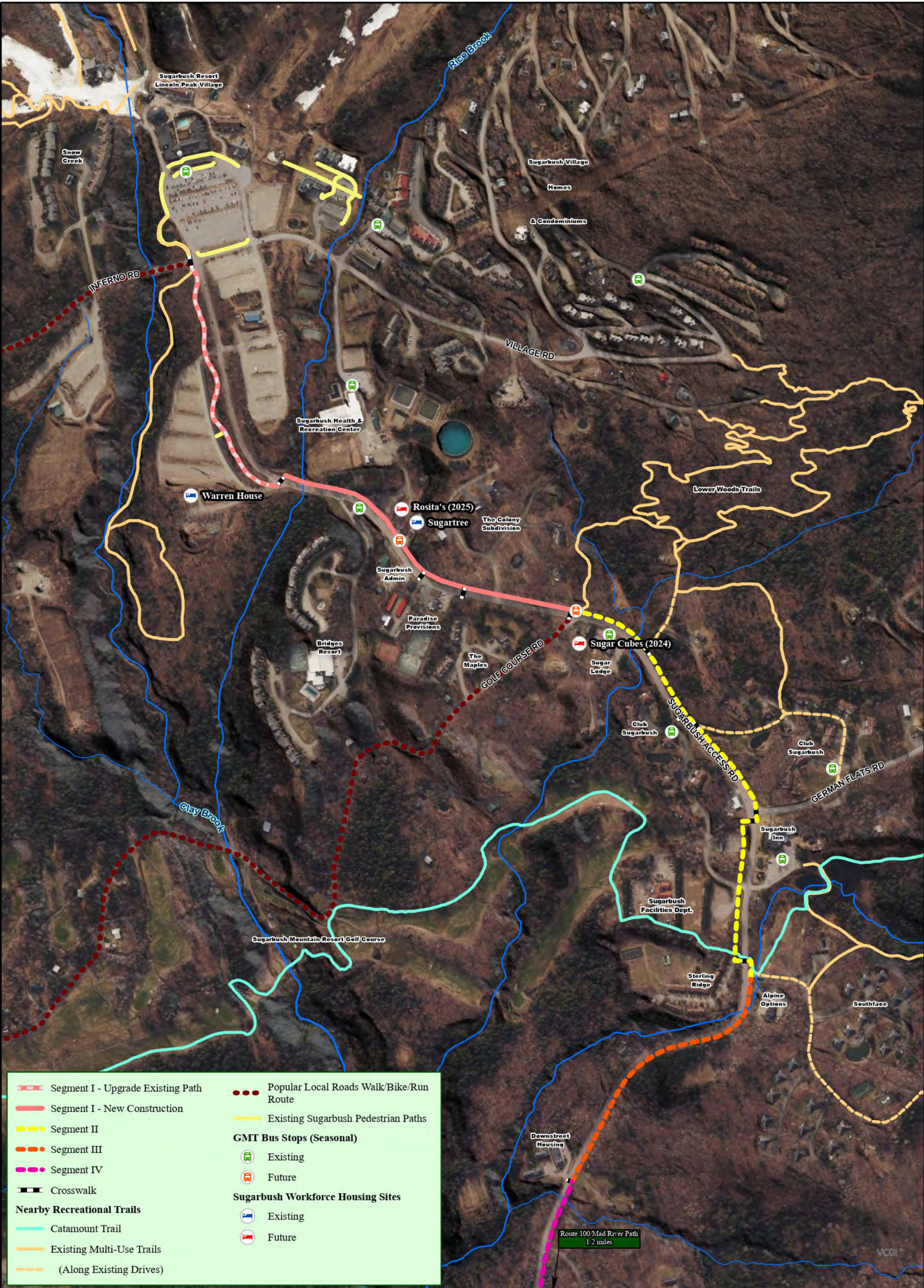


Sugarbush Access Road Shared Use Path - Map 3
Preferred Segment I Alignment with Adjacent Parcel Owners
Warren, Vermont

Prepared by Sugarbush Resort Planning Dept., June 2023

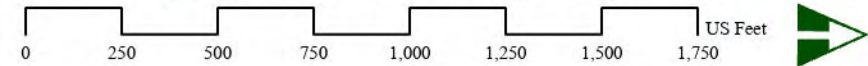


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Municipal Building
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Warren, Vermont
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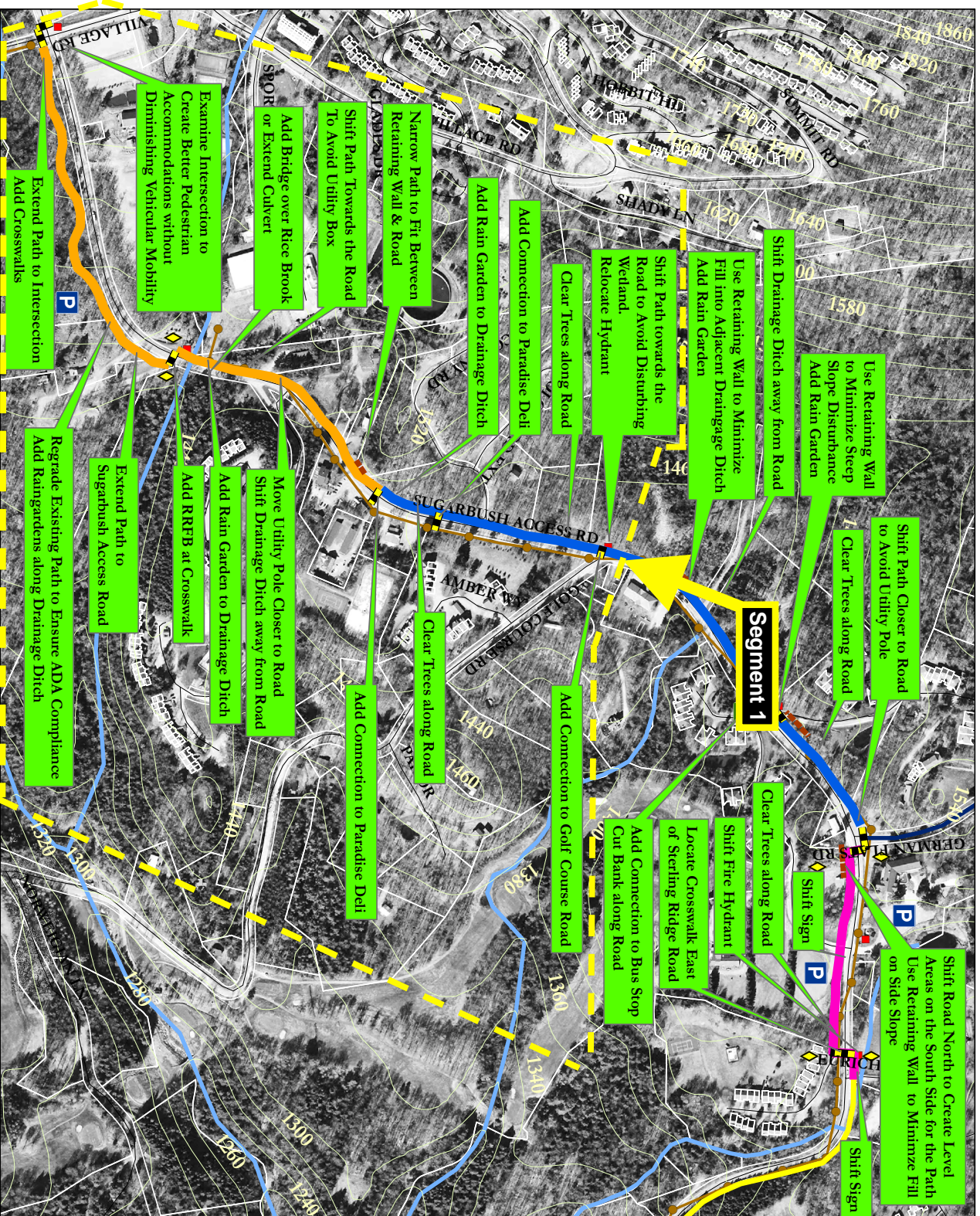


Sugarbush Access Road Shared Use Path - Map 2
Preferred Segments I - IV with Existing Trails & Nearby Facilities
Warren, Vermont

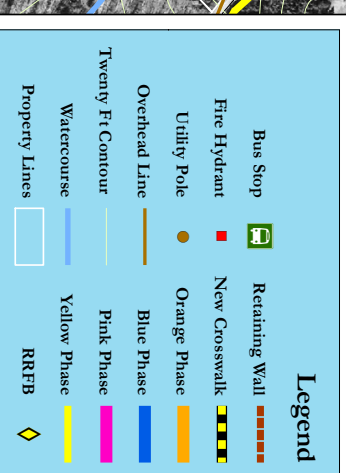
Prepared by Sugarbush Resort Planning Dept., June 2023



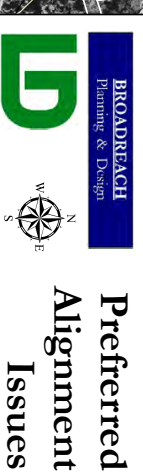
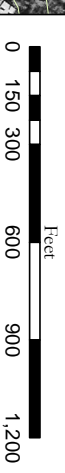
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Sugarbush Access Road Bicycle & Pedestrian Scoping Study Warren, Vermont



Segment 1



Heritage Landscapes
Preservation, Landscape Architecture, & Planning
February 7, 2017

Page 40
Figure 3

**Town of Warren | Sugarbush Access Road ADA Accessible Shared-Use
Path, Segment 1 (Inferno Rd to Golf Course Rd)**

June 2023 Budget

Total Costs Break-down	
Construction Items	\$618,380
Contingency	\$92,757
Engineering	\$92,757
ROW	\$6,184
Project Management	\$58,746
Construction Oversight	\$92,757
Total	\$961,580

Total Grant Request	Total Match (20%)
\$769,264	\$192,316

Segment 1 Cost Break-down by Scoping Study Phases

- Orange Phase (Inferno Rd. to Paradise Deli)
- Blue Phase, 775' (Paradise Deli to Golf Course Rd.)

Orange Phase Total Cost	Blue Phase (775') Total Cost	Orange + Blue Phases Total Cost
\$825,336	\$136,245	\$961,580

This budget is built off of what was included in the [Sugarbush Access Road Path Scoping Study \(2017\)](#), updated with *VTrans 2-Year Weighted Bid Averages (2021-2023)* and referencing *VTrans January 2020 Report on Multi-Use Path and Sidewalk Costs*. Budget completed by Joshua Schwartz, Executive Director of the Mad River Valley Planning District, in consultation with Doug Henson, Survey Project Manager with Trudell Consulting Engineers (previous Vice-President of Lamoureux & Dickinson and project principle on the 2017 Scoping Study).

Budget Detail

Inferno Rd to Paradise Deli (Orange Phase)				
<i>6-6-2023 update per VTrans 2-Year Weighted Bid Averages (2021-2023) & VTrans January 2020 Report on Multi-Use Path and Sidewalk Costs</i>				
Item	Unit	Quantity	Unit cost	Extended cost
Common Excavation (203.15)	CY	950	\$18	\$17,005
Ten Foot Wide Pavement (406.35)	T	350	\$94	\$32,900
Island / Road Widening	LS	1	\$75,000	\$75,000
Fine Graded Crushed Stone (301.26)	CY	1700	\$61	\$103,003
Uniformed Traffic Officers (630.10)	HR	40	\$71	\$2,827
Flaggers (630.15)	HR	160	\$40	\$6,395
Mobilization / Demobilization	LS	1	\$51,650	\$51,650
Traffic Control	LS	1	\$2,000	\$2,000
Painted Crosswalk (646.311)	LF	72	\$9	\$671
Rain Garden	EA	2	\$15,000	\$30,000
RRFB Per Crosswalk	Set	1	\$15,000	\$15,000
Detectable Warning Surface (618.30)	SF	43	\$55	\$2,361
Traffic Signs	SF	40	\$20	\$800
Cast in Place Foundation	EA	2	\$15,000	\$30,000
Eleven Foot Wide Bridge	SF	1100	\$135	\$148,500
Retaining Wall	SY	500	\$12	\$6,000
Trees (656.20)	EA	4	\$478	\$1,914
24" Culvert (601.2620)	LF	50	\$95	\$4,737
Sub Total				\$530,763
Contingency @ 15%	Est.	1	\$79,614	\$79,614
Engineering @ 15%	Est	1	\$79,614	\$79,614
ROW @ 1%	Est	1	\$5,308	\$5,308
Project Management @ 9.5%	Est	1	\$50,422	\$50,422
Construction Inspection @ 15%	Est	1	\$79,614	\$79,614
TOTAL				\$825,336

Paradise Deli to Golf Course Road (775' of Blue Phase)				
<i>6-6-2023 update per VTrans 2-Year Weighted Bid Averages (2021-2023) & VTrans January 2020 Report on Multi-Use Path and Sidewalk Costs</i>				
Item	Unit	Quantity	Unit cost	Extended cost
Common Excavation (203.15)	CY	290	\$18	\$5,191
Ten Foot Wide Pavement (406.35)	Ton	107	\$94	\$10,027
Fine Graded Crushed Stone (301.26)	CY	517	\$61	\$31,305
Uniformed Traffic Officers (630.10)	HR	20	\$71	\$1,413

Flaggers (630.15)	HR	67	\$40	\$2,665
Mobilization / Demobilization	LS	1	\$13,803	\$13,803
Traffic Control	LS	1	\$4,000	\$4,000
Painted crosswalk (646.311)	LF	50	\$9	\$466
Rain Garden	EA	1	\$15,000	\$15,000
Detectable Warning Surface (618.30)	SF	40	\$55	\$2,200
Traffic Signs	SF	30	\$20	\$600
24" culvert (601.2620)	LF	10	\$95	\$947
Sub Total				\$87,617
Contingency @ 15%	Est.	1	\$13,143	\$13,143
Engineering @ 15%	Est	1	\$13,143	\$13,143
ROW @ 1%	Est	1	\$876	\$876
Project Management @ 9.5%	Est	1	\$8,324	\$8,324
Construction Inspection @ 15%	Est	1	\$13,143	\$13,143
TOTAL				\$136,245

(5) Letter of Support from Central Vermont Regional Planning Commission



May 19, 2023

Peter Pochop, Project Manager
VTrans Municipal Assistance Section
Vermont Agency of Transportation
219 North Main Street
Barre, VT 05641

Dear Peter Pochop,

The Central Vermont Regional Planning Commission is pleased to support the Town of Warren's application for VTrans 2023 Bike and Pedestrian Program funding. It is our understanding that the Town will initiate the design/construction for a phased approach to building a new ADA accessible multi-use path to connect major destinations, residential and commercial land uses, and transit stops along the Sugarbush Access Road.

The Regional Plan, encourages addressing gaps in the bike and pedestrian network while linking residential neighborhoods with commercial destinations and other areas of interest. The following regional policies are directly relevant to the development of this project:

- Integrate modes of travel in order to allow for their most effective use and ultimately reduce dependence on single occupant vehicles, including:
 - Ensure adequate mobility for all segments of the population, including residents who cannot or do not use private automobiles.
 - Encourage the availability of multiple options for the movement of people and goods.
- Establish a transportation system that minimizes consumption of resources and maximizes the protection of the environment.
- Promote projects that limit the conflicts between the motor vehicle traffic stream, pedestrians, and the rail system.

For the reasons listed above, the improved access for worker housing and access to transit, the support of local economic growth, and the improved non-motorized safety, the Central Vermont Regional Planning Commission is pleased to support this project.

Sincerely,

Keith Cubbon

Keith Cubbon, Planner

29 Main Street Suite 4 Montpelier Vermont 05602
802-229-0389 E Mail: CVRPC@CVRegion.com

(6) Letter of Commitment from the Town of Warren

TOWN OF WARREN
MUNICIPAL BUILDING
PO BOX 337
Warren, Vermont
05674-0337

Mr. Peter Pochop, Project Manager
Project Manager VT Agency of Transportation
Municipal Assistance Bureau
1 National Life Drive
Montpelier, VT 05633

June 6, 2023

RE: Town of Warren | 2023 Federal Aid Bicycle & Pedestrian Grant Application

Dear Mr. Pochop:

This letter supports the Town of Warren's application for Bicycle & Pedestrian Grant funding for final design and construction of the Sugarbush Access Road ADA Accessible Shared-Use Path between Inferno Road and Golf Course Road in Warren, Vermont. This project represents "Segment 1" of the Town of Warren's Sugarbush Access Road Path Scoping Study funded through the VTrans Bicycle & Pedestrian Program (Warren STP BP15(13)). The project exemplifies a true collective endeavor and strengthens the community via the efforts of many partners within the Mad River Valley. The Town of Warren is proud to work with the Mad River Valley Planning District, Mad River Path Association, and Sugarbush Resort in submitting this application.

The Town of Warren commits \$192,316 to the project, representing a 20% local match. Upon the Shared-Use Path's completion, the Town of Warren commits to future maintenance responsibility to ensure continued enjoyment by the community during all four seasons.

As our application states, this Shared-Use Path will serve many different purposes in our community. It is something the Town of Warren has envisioned for many years and the 2023 Bicycle & Pedestrian Grant provides hope that Warren can offer such a significant asset to the people of the Town and the many guests that visit the Town of Warren.

We, as community members, hope that the VT Agency of Transportation will look favorably upon the Town of Warren's grant application for the Sugarbush Access Road ADA Accessible Shared-Use Path, Segment 1.

Warren Selectboard

Robert M Ackland

Robert Ackland, Selectboard Member

(7) Documentation of contract with VTrans District Office, if the project is on the State system

N/A, as the project is not on the state system.

(8) Supporting Documentation (scoping study or equivalent report, maps, and drawings)

Note: If the scoping study is in a publicly accessible location online, applicants may provide a link with reference to relevant pages as appropriate.

1. Sugarbush Access Road Path Bicycle and Pedestrian Scoping Study
[Warren STP BP15(13)]
 - a. Final Study/Plan Document
 - i. https://mrvpd.org/wp-content/uploads/2019/06/Sugarbush-Access-Road-Path-Scoping-Study_Warren-STP-BP1513-Final-Report_2.22.17.pdf
 - b. Final Study/Plan Maps
 - i. https://mrvpd.org/wp-content/uploads/2019/06/Sugarbush-Access-Road-Scoping-Study_Figures_2.7.17.pdf
2. Additional Letters of support, below
 - a. Sugarbush
 - b. Mad River Path Association
 - c. Green Mountain Transit
 - d. Downstreet Housing & Community Development
 - e. Mad River Valley Planning District
 - f. The Colony at Lincoln Peak
 - g. Mad River Valley Chamber of Commerce
3. Project Timeline
4. Mad River Valley Active Transportation Plan
 - a. https://mrvmoves.files.wordpress.com/2016/12/mrv-active-transportation-plan-doc_final_opt.pdf
5. Mad River Valley – Economic Study
 - a. https://cdn.townweb.com/mrvpd.org/wp-content/uploads/2019/07/MRVPD_ECONSTUDY_FINAL_JUNE2014.pdf

(2a) Sugarbush – Letter of Support



Mr. Peter Pochop, Project Manager
VT Agency of Transportation
Municipal Assistance Bureau
219 North Main Street
Barre, VT 05641

June 2, 2023

Re: 2023 Town of Warren Bicycle & Pedestrian Grant Application

Dear Mr. Pochop:

Sugarbush Resort is excited to submit this letter of support for the Town of Warren's application for Bicycle & Pedestrian Grant funding for final design and construction of the Sugarbush Access Road Shared Use Path Project between Inferno Road and Golf Course Road in Warren, Vermont. The project will involve continuing the partnership between the town, the Mad River Valley Planning District, Mad River Path Association, and Sugarbush Resort, along with other MRV trail and recreation organizations and property owners along the Sugarbush Access Road. Sugarbush has been an active participant on the Mad River Valley Trails Collaborative for the past several years and we believe enhancing our active transportation facilities within the Mad River Valley benefit residents and visitors alike. The Shared Use Path will provide an important multi use route along with access to safe and fun paths and trails, support healthy lifestyles, and support the Mad River Valley's four-season recreation economy.

Sugarbush partnered with the town on, and contributed a portion of the local match for, the 2017 Sugarbush Access Road Path Scoping Study, which was funded through a Bicycle & Pedestrian Program grant. We are again committed to working with the town and project partners and have contributed \$200,000 towards the local match of the Town of Warren's Bicycle & Pedestrian Grant application for the purpose of supporting this project through final design and implementation. We also commit to granting permanent rights of way for the path and allow project construction staging, temporary slope and construction easements to occur on resort lands as needed. Several sections of the proposed path alignment cross resort land. One of these sections follows the existing gravel Sugarbush Resort Parking Lot Path adjacent to, and south of, the Sugarbush Access Road. Sugarbush supports the enhancement of the existing gravel path to meet ADA requirements and will seek amendments to our existing Conditional Use, Act 250 and Stormwater permits if amendments are required by the town and/or state. We are pleased to continue participating with this public-private partnership which demonstrates our willingness to work together to support recreation that supports long-term vitality of the town, resort and greater Mad River Valley community.

We urge the VT Agency of Transportation to review the town's grant application for the Sugarbush Access Road Shared Use Path favorably. This multi-use facility will make the Sugarbush Access Road safer and provide a key active transportation link for the town and surrounding area.

Sincerely,

A handwritten signature in blue ink, appearing to read "John Hammond", is written over the word "Sincerely,".

John Hammond,
President and COO

cc: Town of Warren
Misha Golfman, MRPA
Joshua Schwartz, MRVPD

(2b) Mad River Path Association – Letter of Support



Peter Pochop, Project Manager
VTrans Municipal Assistance Section
Vermont Agency of Transportation
219 North Main Street
Barre, VT 05641

May 23, 2023

Dear Peter,

I am writing this letter in support of the town of Warren Application for Phase I of the Sugarbush Access Road ADA-accessible shared use path.

In its May 10 meeting, the MRPA board of directors expressed its support for the Sugarbush Path and authorized MRPA to cooperate with partners in support of this important initiative. The number one reason the MRPA is excited about the completion of the first phase of this project is because it provides safe pedestrian access for 240 seasonal and year-round workers who will be housed at several workforce units along the proposed route. Many of these workers don't have access to personal transportation and walk daily to and from their work at Sugarbush. The path will provide safe alternative transportation and contribute to the quality of life of the workers. The second equity component that the MRPA is equally excited about is that we believe, that this first phase of the path will literally "pave the way" for the subsequent phases that will bring the path to Downstreet's Wheeler Brook affordable housing development, a short distance down the Sugarbush Access Road. Connecting this housing complex with the ski resort via the path will provide its residents with an outstanding opportunity, allowing children, teens, and adults to access the mountain with its limitless recreational resources on foot in all four seasons! The next important function of this path is safe access to Green Mountain Transit, Mad Bus for all users. The active recreational loop follows the Golf Course, West Hill, Inferno, and Access roads. The proposed path section will divert the existing recreational traffic off the busy Sugarbush access road. Finally, the proposed path will create another trail connection to the existing Lower Woods trails network.

We are encouraged to see that Sugarbush Resort is working to create better working conditions for its workforce and are excited to support their efforts by supporting an accessible multi-use path that will enhance the quality of life for all its users.

Respectfully,



Misha Golfman,
Executive Director
Mad River Path Association
603 903 5567; misha@madriverpath.org



June 2, 2023

Mr. Peter Pochop, Project Manager
VT Agency of Transportation
Municipal Assistance Bureau
219 North Main Street
Barre, VT 05641

RE: 2023 Town of Warren Bicycle & Pedestrian Grant Application
Sugarbush Access Road Shared Use Path Project

Greetings:

Green Mountain Transit (GMT) is excited to submit this letter of support for the Town of Warren's application for Bicycle & Pedestrian Grant funding for final design and construction of the Sugarbush Access Road Shared Use Path Project between Inferno Road and Golf Course Road in Warren, Vermont.

GMT has been operating winter season Mad Bus public bus service in the Mad River Valley (MRV) since 2003. The Mad Bus system has four bus routes, of which three of these routes travel along the Sugarbush Access Road. The addition of a shared use path along the Sugarbush Access Road will improve safe walking conditions along the road and enhance recreation opportunities for the community.

GMT is happy to support the project via adding and/or relocating bus stop locations along the route to facilitate safe and convenient access to the path and to the bus system. GMT has been a member of the Mad River Valley Transportation Advisory Committee since 2003 and works closely with the Mad River Valley Planning District, Sugarbush Resort, the towns of Fayston, Warren and Waitsfield, and other community organizations to provide convenient and reliable public transportation in the MRV. Through the MRV Transportation Advisory Committee we commit to working with the project team to update bus stop locations to best support the new path and Mad Bus system users.

We encourage the Vermont Agency of Transportation to approve the town's request for grant funding to create a path along the Sugarbush Access Road



to improve safety, increase shared use connections and support outdoor recreations in the area.

Sincerely,

A handwritten signature in black ink that reads 'Jamie Smith'.

Jamie Smith,
Director of Planning and Marketing
Green Mountain Transit

cc: Town of Warren
Joshua Schwartz, MRVPD

(2d) Downstreet Housing & Community Development – Letter of Support



22 Keith Avenue, Suite 100
Barre, Vermont 05641
Downstreet.org • (802) 476-4493

6/1/2023

Peter Pochop, Project Manager
VTrans Municipal Assistance Section
Vermont Agency of Transportation
219 North Main Street
Barre, Vermont 05641

RE: Letter of Support for Funding Sugarbush Access Road Shared Use Path

Dear Peter,

Downstreet Housing & Community Development is pleased to support the Town of Warren's application for Phase 1 of the Sugarbush Access Road Shared Use Path. Phase 1 of this project will be connecting Sugarbush Resort to the greater Warren community, creating important pedestrian access for Sugarbush Resort's seasonal and year-round workers. Phase 2 will extend the path all the way to Downstreet's 18-unit affordable housing development- Wheeler Brook, connecting residents to town amenities and providing a form of outdoor recreation. It is because this path will increase walkability for members of Vermont's workforce and eventually Downstreet's residents, that we strongly support the proposed creation of the Sugarbush Access Road Shared Used Path.

Phase 1 of this project will provide 140 seasonal and year-round workers with safe pedestrian access to their employer- Sugarbush Resort, while providing a form of outdoor recreation for these residents. Many of these workers do not currently have personal transportation, and are walking to and from their work at Sugarbush on a daily basis. We are excited about the creation of this new path which will increase the safety and quality of life for these individuals.

Paving the way for Phase 2 of this path, the Sugarbush Access Road Shared Use Path will eventually connect to Downstreet's Wheeler Brook affordable housing development, located a few hundred yards down from the Sugarbush Access Road. Downstreet is always looking at options to provide outdoor recreation for our residents, with this path presenting a great opportunity to provide children, teens, and adults a variety of outdoor recreation options for all four seasons.

For all of the reasons stated above, Downstreet Housing & Community Development supports the Sugarbush Access Road Shared Use Path. We look forward to having this investment in Warren that will further walkability for Sugarbush Resort's seasonal and year-round workers and will promote outdoor recreation for Downstreet's residents along with the greater Warren community.

Sincerely,

A handwritten signature in blue ink that reads "Nicola Anderson".

Nicola Anderson
Director of Real Estate Development



(2e) Mad River Valley Planning District – Letter of Support



June 1, 2023

Mr. Peter Pochop, Project Manager
Project Manager VT Agency of Transportation
Municipal Assistance Bureau
1 National Life Drive
Montpelier, VT 05633

Re: VTrans Bicycle & Pedestrian Program | Sugarbush Access Road Shared Use Path,
Segment I

Dear Mr. Pochop,

Please be advised that the Mad River Valley Planning District (MRVPD) wholeheartedly supports the Town of Warren's application to the Bicycle and Pedestrian Program to fund Design/Construction of the Sugarbush Access Road Shared Use Path, Segment I.

Walkability is a key ingredient to the success and safety of the Mad River Valley's resort areas. The proposed project will implement a phased approach to construct an ADA-accessible path, greatly improving pedestrian and bicycle safety and closing a key active transportation gap in this town-identified growth center. We also believe that this project will greatly benefit residents of Sugarbush's upcoming workforce housing projects, who often have no access to private transportation. This project is the first step in connecting the Sugarbush Village area with residents of the Downstreet Housing & Community Development's Wheeler Brook housing development. As residential building continues in this area, expansion of accessible pedestrian and bicycle infrastructure is vital and will provide an important enhancement for both safety and access to recreation for the area's residents and visitors. This project serves as an important leverage point for the community that will address issues of pedestrian safety, improve access to transportation for underserved communities, increase local economic growth, and expand recreational opportunity.

MRVPD continues to be a collaborative and engaged partner in Warren's infrastructure enhancements. We are committed to providing the necessary resources and skills in direct support of Warren's management of this project, serving a role similar to what we provided during the Bicycle & Pedestrian Program funded Sugarbush Access Rd. Path Scoping Study (2015-17). MRVPD is excited to help ensure this long-awaited community project comes to reality.

MRVPD is proud of Warren's dedication to this project and sees this application as a way to further increase the Mad River Valley's transportation opportunities toward a safe, multimodal, and connected future.

Sincerely,

A handwritten signature in blue ink, appearing to read 'Josh Schwartz', written over a light blue horizontal line.

Joshua Schwartz
Executive Director

The Mad River Valley Planning District was created in 1985 by the Towns of Fayston, Waitsfield and Warren. The Purpose of the Planning District is to "carry out a program of planning for the future of the Mad River Valley. The planning program shall be directed toward the physical, social, economic, fiscal, environmental, cultural and aesthetic wellbeing of the member Towns and its inhabitants."

**The Colony at Lincoln Peak
Joseph P. Hanley, President
30 Colony Road Extension
P.O. Box 163, Warren, VT 05673**

June 6, 2023

Mr. Peter Pochop, Project Manager
VT Agency of Transportation
Municipal Assistance Bureau
219 North Main Street
Barre, VT 05641

Re: Support for Bicycle & Pedestrian Grant Application – Sugarbush Access Rd., Warren

Dear Mr. Pochop:

On behalf of the Board of Directors of the Colony at Lincoln Peak, a 9-lot sub-division off the Sugarbush Access Road in Warren, VT (the “Colony”), I am writing to express our **support** for the Town of Warren’s Bicycle & Pedestrian Grant funding application to design and construct the Sugarbush Access Road Shared Use Path, between Inferno and Golf Course Roads and along this section of the Sugarbush Access Road in Warren (the “Shared Path Project”).

The Colony’s sub-division includes approximately 950 feet of frontage along the northern side of the Sugarbush Access Road (the “Access Road”), which aligns with the preferred location of the Shared Path Project at this section. As part of its sub-division approval by the Town of Warren in 2008, the developer granted a pedestrian easement to accommodate this portion of the Shared Path Project (*see attached decision and subdivision survey*).

The Shared Path Project would dramatically improve pedestrian connectivity at this gateway location at the top of the Access Road, by creating a safe and vibrant public accessway to and from the base of Sugarbush Resort at Lincoln Peak (the “Resort”). Although part of an expanded and improved community of new residents, employees, local retail and hospitality options, this area at the top of the Access Road is not safely accessible on foot. As a result, vehicle usage is the dominant form of travel at this location, which conflicts with pedestrian safety for those who attempt to walk along this section of the Access Road (especially at night).

The Colony is a newer sub-division which is almost fully built-out (8 of our 9 lots are now improved with single-family homes). We include a vibrant mix of full-time residents and second homeowners, many of whom routinely shop at the neighboring Paradise market and frequent the new bar at the improved Sugar Lodge (both directly across the Access Road), in addition to the Resort’s year-round amenities. Safe pedestrian walkability would dramatically improve the health, safety and vibrancy of this gateway location, consistent with modern planning principles for an improved environment, with less reliance on vehicle usage and a healthier public realm. It would also strengthen the sense of community and benefit the public.

Mr. Peter Pochop, PM
VT Agency of Transportation
June 6, 2023

For these reasons, we enthusiastically support the Town's application and hope that your agency will vote to fund this special opportunity to improve pedestrian safety, increase shared use connections and support expanded public access to outdoor recreation in the area.

Very truly yours,

A handwritten signature in black ink, appearing to read "Joseph P. Hanley". The signature is fluid and cursive, with a large initial "J" and a stylized "H".

Joseph P. Hanley, President

Enclosures: Town Decision and Survey

cc: Holly Grant (Treasurer and Clerk), Dexter LeFavour (Vice President) and Harvey Blake (Member)

**TOWN OF WARREN
DEVELOPMENT REVIEW BOARD
FINDINGS OF FACT AND NOTICE OF DECISION
#2006-15-SD LBO LLC 9-LOT SUBDIVISION**

Michael Krongel, representing LBO, LLC has submitted an application for a 9-lot residential subdivision located on the Sugarbush Access Road, parcel id # 005008-301, in the Vacation Residential District. The property consists of approx. 16+/- acres.

Duly warned hearings were held on August 23, 2006, October 18, 2006, December 20, 2006, January 31, 2007 and March 7, 2007. A site visit was also conducted on Saturday December 2, 2006.

August 23, 2006: Members in attendance: Peter Monte, David Markolf, Lenord Robinson, Virginia Roth and Chris Behn. Others in attendance were: Shelia Getzinger, Jennifer Grace, Robert Riversong, Mike Krongel, Don Marsh, Zeke Church, Nancy Segal, Don Swain, Mark Bannon, June Sardi, Miron Malboeuf and Ruth Robbins.

October 18, 2006: Members in attendance: Peter Monte, David Markolf, Chris Behn, Virginia Roth and Lenord Robinson. Others in attendance were: Mike Krongel, Don Marsh, Larry Ryan, Erin Post, Dick King, Miron Malboeuf and Ruth Robbins.

December 2, 2006 Site Visit: Members in attendance: Chris Behn, Peter Monte, David Markolf and Virginia Roth. Others in attendance were: Mike Krongel, Steve Butcher, Jason Lisai, Charles Grenier and Mr. Malboeuf.

December 20, 2006: Members in attendance: David Markolf, Chris Behn and Lenord Robinson. Others in attendance were: Mike Krongel, Don Marsh, Bob Ackland, Jason Lisai, Miron Malboeuf and Ruth Robbins.

January 31, 2007: Members in attendance: Peter Monte, Bob Kaufmann, Lenord Robinson and Chris Behn. Others in attendance were: Mike Krongel, Don Marsh, Jim Caffry, Will Senning, Erin Post, Miron Malboeuf and Ruth Robbins.

March 7, 2007: Members in attendance: Peter Monte, David Markolf, Virginia Roth and Chris Behn. Others in attendance were: Don Marsh, Mike Krongel, Erin Post, Chris Peltier, Cindy Carr, Miron Malboeuf and Ruth Robbins.

Findings of Fact and Conclusions of Law:

- A. The following documents were submitted in conjunction with this application:
1. Completed application, project narrative, abutter letter and proof of mailing and "concept" site plan dated 6/15/06 prepared by Charles Grenier.
 2. Revised site plan dated 7/18/06 prepared by Charles Grenier.
 3. Revised site plan dated 10/9/06 prepared by Charles Grenier.
 4. Proposed Declarations of Covenants, Conditions & Restrictions; proposed By-Laws for The Colony at Lincoln Peak.
 5. Stormwater Runoff Analysis & Narrative
 6. Erosion Prevention & Sediment Control Narrative
 7. Maps dated 12/13/06 by Charles Grenier titled: Stormwater Management Plan, Construction Plan and Erosion Control & Stormwater Details.
 8. "Resource Plan" dated 6/15/06 by Charles Grenier – 2 maps – one with wetlands, one without.
 9. Future Town Road Analysis prepared by Charles Grenier dated 1/17/07
 10. Town Roadway Options – 2 maps, dated 1/17/07 prepared by Charles Grenier.
 11. Revised Declarations and Covenants submitted 2/23/07

12. Request from Don Marsh for front yard setback relief for Lot #4, dated 2/22/07
 13. Letter from the Warren Fire Dept. dated 12/20/06
 14. Maps submitted by Charles Grenier dated 2/20/07 and titled as follows:
 - Overall Site Plan/Subdivision Plan
 - Septic System Details
 - Road Profiles & Details
 - Pre-Construction Plan
 - Construction Plan
 - Erosion Control & Stormwater Details Plan
 - Final Stabilization Plan
 - Stormwater Management Plan
 15. Letter to Don Marsh, Marsh Engineering, dated 12/7/06 from Jason Lisai, Sugarbush Resort re; Fire hydrant for the Colony at Lincoln Peak.
 16. Letter from Michael Krongel to Cindi Jones and Miron Malboeuf dated 12/7/06 re: proposed access road through LBO, LLC property.
-
- B. The parcel is 16 +/- acres of wooded land with some wetlands
 - C. The size of the parcel would allow density of up to 15 units but the applicant felt that land constraints only allowed for nine.
 - D. A pond for storm water would be constructed in the northeast corner of the lot.
 - E. All the building envelopes were placed so as to avoid the steeper slopes.
 - F. A meeting with the Fire Dept resulting in their being comfortable with the plan but requesting two (2) fire hydrants. These hydrants could be hooked up to the Sugarbush snowmaking line, with Sugarbush's permission.
 - G. The wastewater systems are all individual systems, located in a clustered fashioned with some requiring easements from a neighboring lot. All the lots have individual wells.
 - H. The plan is to clear only the obvious deadwood and small saplings, leaving any trees of significance for later determination by the future owner's architect and/or builder.
 - I. Extensive discussion took place (DRB meeting of 12/20/06) about the Town's desire to have future access through the applicant's property as a possible secondary access road to the Sugarbush Village area.
 - J. The Board is requiring that the applicant extend the 50-foot right of way for their development road all the way to the western boundary with Summit Ventures (parcel id 250030-000) and be reflected on the final plat. The last segment beyond what is proposed does not need to be constructed, only shown as a right-of-way on the plat.
 - K. Due to the extended right-of-way, the applicant requests setback relief for the front setback requirement for lot #4.
 - L. It was brought to the attention of the Board that both lot 4 and lot 5 did not meet the 150-foot frontage requirement, as they were located at the end of their respective roads.

- M. To retain as much of the vegetation/trees as possible along the Access Road side of the property a "buffer" of 40' was discussed as being adequate.
- N. The Board requested that the covenants additionally include the following: notice to homeowners that they are required to obtain all applicable Town and State permits prior to building, and maintenance agreement including but not limited to the road, any shared or common areas or utilities and the fire hydrants and water line.
- O. The applicant has submitted the road names of Colony Road and Colony Road Extension.
- P. The applicant suggested that the walkway along the Access Road be placed along the road side of the right-of-way versus the LBO property side of the right-of-way, as it would reduce the number of trees that would have to be cut.
- Q. The Fire Dept. recommends that wood-burning sources be used only for back up heat and constructed with masonry chimneys
- R. Items #3 and #5 in the Fire Dept. letter of 12/20/06 have been reflected on the site plan submitted by the applicant.

Notice of Decision:

The Development Review Board hereby grants the applicant's request for front setback relief for Lot #4 up to the maximum of 30% as allowed under Section 3.6 (C) (1).

The Development Review Board also finds the application satisfies the requirements as called for under Article 6 Subdivision Review and Article 7 Subdivision Standards, hereby approves the applicant's request for a nine-lot subdivision subject to the following conditions:

1. The project shall be constructed and used only in accordance with the terms, plans, procedures and specifications stated:
 - a. In the application
 - b. All exhibits furnished by the applicant
 - c. All findings of fact and conclusions stated above in this decision.The Subdivision Plat approved by the DRB
2. All feeder roads should be 16 feet width plowed in winter. All drives to be 14 feet plowed in winter. The Fire Dept. endorses the current discussions of the Warren Select Board, the Development Review Board and the Planning Commission concerning the creation of a secondary access to Sugarbush Village to serve future growth of the resort area. Such a road might benefit responding fire apparatus in several circumstances. To facilitate this planning, the extension of the applicant's road right-of-way to the west property line is recommended, as well as upgrading the feeder road to town road specifications.
3. Drives to be no less than 30 foot inside radius, both directions.
4. Houses greater than 3,000 square feet shall be constructed with 13D sprinkler systems meeting NFPA standards.
5. The designation a 10-foot pedestrian easement located within the 4-foot right-of-way of the Sugarbush Access Road is to be included on the Final Plat.

**TOWN OF WARREN
DEVELOPMENT REVIEW BOARD
FINDINGS OF FACT & NOTICE OF DECISION – LBO, LLC #2006-15-SD**


In accordance with Section 6.5 and the Act [§4416], within 180 days of the date of receipt of final plan approval under Section 6.4(C), the applicant shall file 3 copies of the final subdivision plat, 1 mylar copy and 2 paper copies, for recording with the town in conformance with the requirements of 27 V.S.A., Chapter 17. Approval of subdivision plats not filed and recorded within this 180-day period shall expire. Prior to plat recording, the plat must be signed by at least two authorized members of the Development Review Board.


Approval shall become effective once this decision has been signed by at least three members of the Warren Development Review Board who participated in the final decision. Beginning the effective date of this decision, there is a 30-day period during which parties may appeal the decision to the Environmental Court.

Development Review Board

Peter Monte _____ date

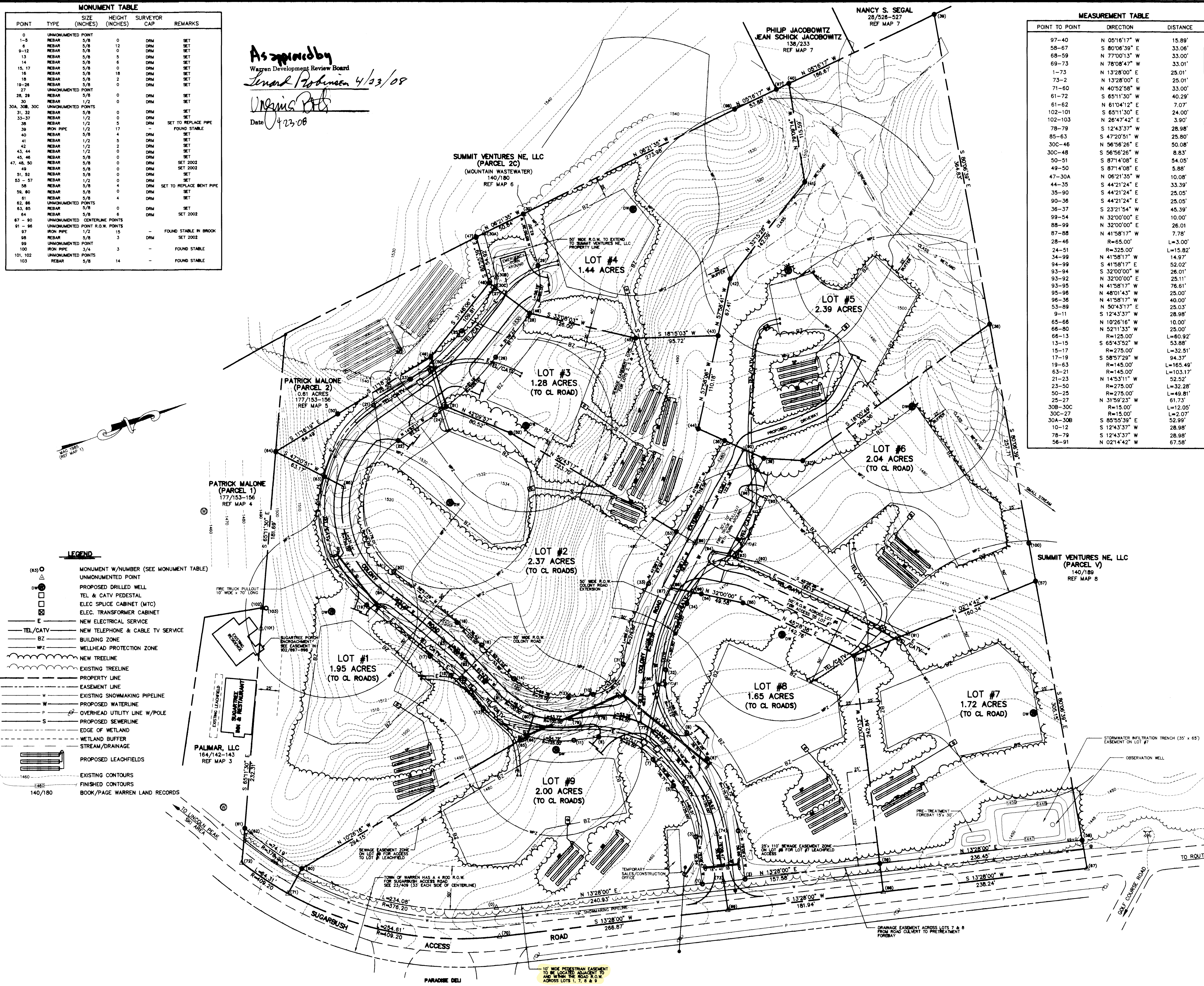
 04-04-07
Chris Behn _____ date

 4/5/07
David Markolf _____ date

 4/4/07
Virginia Roth _____ date

MONUMENT TABLE					
POINT	TYPE	SIZE (INCHES)	HEIGHT (INCHES)	SURVEYOR CAP	REMARKS
0	UNMONUMENTED POINT				
1-5	REBAR	5/8	12	DRM	SET
6	REBAR	5/8	12	DRM	SET
8-12	REBAR	5/8	0	DRM	SET
13	REBAR	5/8	5	DRM	SET
14	REBAR	5/8	5	DRM	SET
15, 17	REBAR	5/8	0	DRM	SET
16	REBAR	5/8	18	DRM	SET
18	REBAR	5/8	2	DRM	SET
19-28	REBAR	5/8	0	DRM	SET
27	UNMONUMENTED POINT				
28, 29	REBAR	5/8	0	DRM	SET
30	REBAR	1/2	0	DRM	SET
30A, 30B, 30C	UNMONUMENTED POINTS				
31, 32	REBAR	5/8	0	DRM	SET
33-37	REBAR	1/2	0	DRM	SET
38	REBAR	1/2	5	DRM	SET TO REPLACE PIPE
39	IRON PIPE	1/2	17		FOUND STABLE
40	REBAR	5/8	4	DRM	SET
41	REBAR	1/2	5	DRM	SET
42	REBAR	1/2	2	DRM	SET
43, 44	REBAR	1/2	0	DRM	SET
45, 46	REBAR	5/8	0	DRM	SET
47, 48, 50	REBAR	5/8	0	DRM	SET 2002
49	REBAR	5/8	0	DRM	SET 2002
51, 52	REBAR	5/8	0	DRM	SET
53-57	REBAR	1/2	0	DRM	SET
58	REBAR	5/8	4	DRM	SET TO REPLACE BENT PIPE
59, 60	REBAR	5/8	0	DRM	SET
61	REBAR	5/8	4	DRM	SET
62, 65	UNMONUMENTED POINTS				
63, 65	REBAR	5/8	0	DRM	SET
64	REBAR	5/8	6	DRM	SET 2002
67-80	UNMONUMENTED CENTERLINE POINTS				
91-96	UNMONUMENTED POINT R.O.W. POINTS				
97	IRON PIPE	1/2	15		FOUND STABLE IN BROOK
98	REBAR	5/8	3	DRM	SET 2002
99	UNMONUMENTED POINT				
100	IRON PIPE	3/4	3		FOUND STABLE
101, 102	UNMONUMENTED POINTS				
103	REBAR	5/8	14		FOUND STABLE

As prepared by
 Warren Development Review Board
 Edward Robinson 4/13/08
 Virginia Pitts
 Date 4/23/08



LEGEND	
(63) Δ	MONUMENT W/NUMBER (SEE MONUMENT TABLE)
DRM	UNMONUMENTED POINT
□	PROPOSED DRILLED WELL
□	TEL & CATV PEDESTAL
□	ELEC. SPICE CABINET (MTC)
□	ELEC. TRANSFORMER CABINET
E	NEW ELECTRICAL SERVICE
TEL/CATV	NEW TELEPHONE & CABLE TV SERVICE
BZ	BUILDING ZONE
WPZ	WELLHEAD PROTECTION ZONE
---	NEW TREELINE
---	EXISTING TREELINE
---	PROPERTY LINE
---	EASEMENT LINE
---	EXISTING SNOWMAKING PIPELINE
---	PROPOSED WATERLINE
---	OVERHEAD UTILITY LINE W/POLE
---	PROPOSED SEWERLINE
---	EDGE OF WETLAND
---	WETLAND BUFFER
---	STREAM/DRAINAGE
---	PROPOSED LEACHFIELDS
---	EXISTING CONTOURS
---	FINISHED CONTOURS
---	BOOK/PAGE WARREN LAND RECORDS

MEASUREMENT TABLE		
POINT TO POINT	DIRECTION	DISTANCE
97-40	N 05°16'17" W	15.89'
58-67	S 80°06'39" E	33.06'
68-59	N 77°00'13" W	33.00'
69-73	N 78°08'47" W	33.01'
1-73	N 13°28'00" E	25.01'
73-2	N 13°28'00" E	25.01'
71-60	N 40°52'58" W	33.00'
61-72	S 65°11'30" W	40.29'
61-62	N 61°04'12" E	7.07'
102-101	S 65°11'30" E	24.00'
102-103	N 26°47'42" E	3.90'
78-79	S 12°43'37" W	28.98'
85-63	S 47°20'51" W	25.80'
30C-46	N 56°56'26" E	50.08'
30C-48	S 56°56'26" W	8.83'
50-51	S 87°14'08" E	54.05'
49-50	S 87°14'08" E	5.88'
47-30A	N 06°21'35" W	10.08'
44-35	S 44°21'24" E	33.39'
35-90	S 44°21'24" E	25.05'
90-36	S 44°21'24" E	25.05'
36-37	S 23°21'54" W	45.39'
99-54	N 32°00'00" E	10.00'
88-99	N 32°00'00" E	26.01'
87-88	N 41°58'17" W	7.78'
28-46	R=65.00'	L=3.00'
24-51	R=325.00'	L=15.82'
34-99	N 41°58'17" W	14.97'
94-99	S 41°58'17" E	52.02'
93-94	S 32°00'00" W	26.01'
93-92	N 32°00'00" E	25.11'
93-95	N 41°58'17" W	76.61'
95-96	N 48°01'43" W	25.00'
96-36	N 41°58'17" W	40.00'
53-59	N 50°43'17" E	25.03'
9-11	S 12°43'37" W	28.98'
65-66	N 10°26'16" W	10.00'
66-80	N 52°11'33" W	25.00'
66-13	R=125.00'	L=60.92'
13-15	S 65°43'52" W	53.88'
15-17	R=275.00'	L=32.51'
17-19	S 58°57'29" W	94.37'
19-63	R=145.00'	L=165.49'
63-21	R=145.00'	L=103.17'
21-23	N 14°53'11" W	52.52'
23-50	R=275.00'	L=32.28'
50-25	R=275.00'	L=49.81'
25-27	N 31°59'23" W	61.73'
30B-30C	R=15.00'	L=12.05'
30C-27	R=15.00'	L=2.07'
30A-30B	S 85°55'39" E	52.99'
10-12	S 12°43'37" W	28.98'
78-79	S 12°43'37" W	28.98'
56-91	N 02°14'42" W	67.58'

SURVEY NOTES

TOTAL STATION SUBDIVISION SURVEY OF LOTS 1-9 BY CHRIS AUSTIN, RYAN MARSH AND DANIEL MULLIGAN IN MAY 2007 AS DIRECTED BY LBO, LLC.

WETLANDS WERE DELINEATED BY PETER SPEAR OF NRCS AND LOCATED BY DANIEL MULLIGAN AND CHRIS AUSTIN IN DECEMBER 2005.

CONTOUR LINES WERE TAKEN FROM REFERENCE MAP 1 INFORMATION AND WERE NOT MAPPED BY THIS OFFICE.

BEARINGS ARE BASED UPON REFERENCE MAP 1 WHICH INDICATES 1985 MAGNETIC NORTH AS A BASIS.

BEARINGS ARE ACCURATE TO THE NEAREST 20" OF ARC, BUT ARE DISPLAYED TO THE NEAREST SECOND OF ARC FOR MATHEMATICAL PURPOSES ONLY.

DISTANCES ARE ACCURATE TO THE NEAREST 0.06', BUT ARE DISPLAYED TO THE NEAREST HUNDREDTH OF A FOOT FOR MATHEMATICAL PURPOSES ONLY.

VARIATIONS IN BEARINGS AND DISTANCES RELATIVE TO THE NOTED REFERENCE SURVEY MAPS IS DUE TO A REFINEMENT IN SURVEY METHOD AND/OR A DIFFERENT BASIS OF BEARINGS.

CALCULATIONS AND DEED RESEARCH BY DANIEL MULLIGAN.

DRAFTING BY TERESA MERRILL.

REFERENCE MAPS

- 1) "WINDRIDGE - ALEXANDER MACDUFF PROPERTY - SUGARBUSH VALLEY ACCESS ROAD - WARREN, VT JANUARY 1985 BY KREBS & LANSING CONSULTING ENGINEERS, INC. AND RECORDED IN MAP BOOK 3 PAGE 52.
- 2) "ALEXANDER MACDUFF PARCELS - ON WESTERLY SIDE OF ACCESS ROAD" JUNE 1985 BY PAUL BIGELOW AND RECORDED IN MAP BOOK 1 PAGE 9A.
- 3) "PROPERTY OF WILLIAM AND JOAN ROGOWSKI - WARREN, VERMONT" APRIL 1980 BY PAUL HARRINGTON AND RECORDED IN MAP BOOK 2 PAGE 160.
- 4) "CONCEPT PLAN - PROPOSED 8 UNIT MULTI-FAMILY HOUSING PROJECT OWNERS: STEVEN & PATRICIA FOWLER - 2500 SUGARBUSH ACCESS ROAD - WARREN, VERMONT" JANUARY 2005 BY CHARLES GRENIER, CONSULTING ENGINEER, PC AND RECORDED IN MAP BOOK 3 PAGE 477.
- 5) "SURVEY OF PORTION OF PROPERTY OF LBO, LLC TO BE CONVEYED TO M.W. ENTERPRISES, INC. - SUGARBUSH ACCESS ROAD - WARREN, VERMONT" MAY 2002 BY CHARLES GRENIER, CONSULTING ENGINEER, PC AND RECORDED IN MAP BOOK 3 PAGE 423.
- 6) "LANDS OF SNOWKIDGE, INC. - SUGARBUSH SPORTS CENTER AND MOUNTAIN WASTE WATER PARCELS IN WARREN, VT" MAY 1995 BY JOHN ROTH AND RECORDED IN MAP BOOK 3 PAGE 203.
- 7) LOTS 100 AND 101 - SUGARBUSH VILLAGE, WARREN, VT MAY 1965 BY JOHN ROTH AND RECORDED IN MAP BOOK 1 PAGE 63A.
- 8) "A PORTION OF THE ROY & MILDRED LONG PROPERTY - WARREN, VERMONT" SEPTEMBER 24, 1982 BY G & S SURVEYORS, INC. AND RECORDED IN MAP BOOK 3 PAGE 54.

TITLE SOURCE:

LBO, LLC FROM SUGARBUSH LAND HOLDINGS INC. BY LIMITED WARRANTY DEED DATED SEPTEMBER 24, 2001 AND RECORDED IN BOOK 140 PAGES 480-482 OF THE TOWN OF WARREN LAND RECORDS.

PERMITS

STATE OF VERMONT, AGENCY OF NATURAL RESOURCES PERMIT WW-5-4022 DATED OCTOBER 21, 2006.

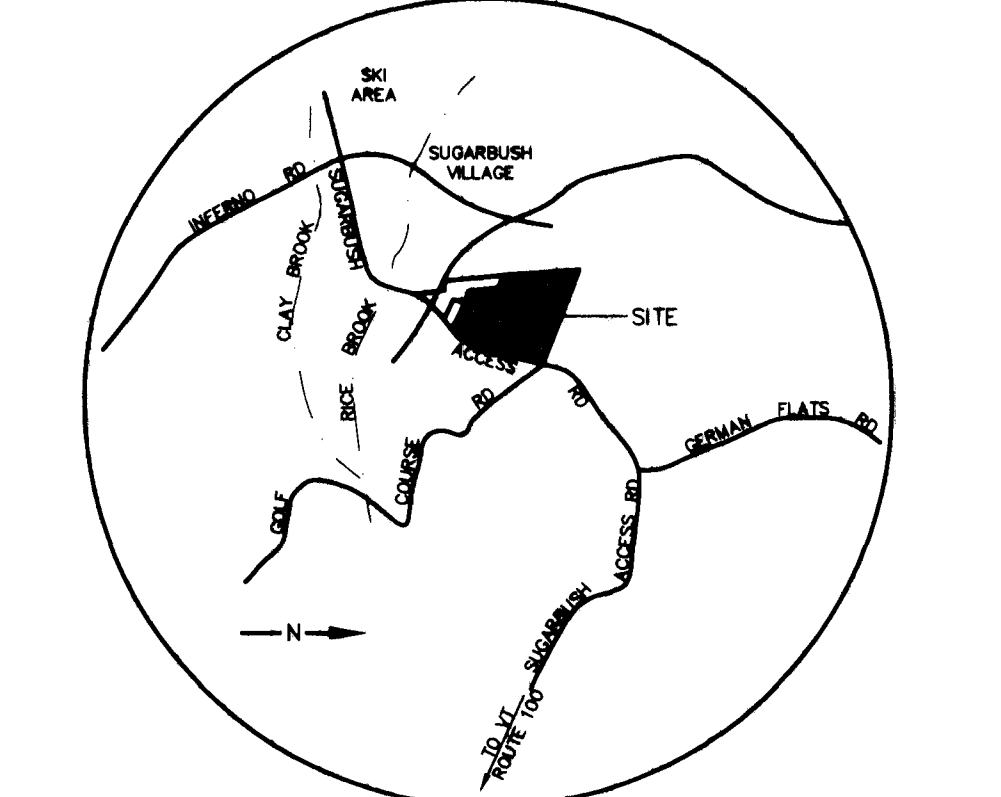
VERMONT DEPARTMENT OF ENVIRONMENTAL CONSERVATION DISCHARGE PERMIT 4879-9015 UNDER GENERAL PERMIT 3-0915 DATED MARCH 15, 2007.

TOWN OF WARREN DEVELOPMENT REVIEW BOARD PERMIT 2006-15-SD DATED APRIL 5, 2007 AND RECORDED IN BOOK 189 PAGES 668-671 OF THE TOWN OF WARREN LAND RECORDS.

TOWN OF WARREN ROAD ACCESS APPROVAL PERMIT NO. 2007-04-RC DATED FEBRUARY 13, 2007.

REVISION MARCH 2008

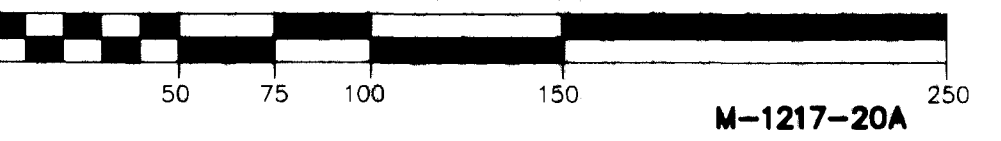
CHANGED LOT 5 PROPOSED DRIVEWAY.



LOCATION MAP

Daniel R. Mulligan
 I CERTIFY THAT THIS SURVEY CONFORMS TO PERTINENT LAND RECORDS AS CONFIRMED OR MODIFIED BY EVIDENCE FOUND ON THE GROUND AND IS CONSISTENT WITH SUCH (EXCEPT WHERE NOTED) AND WAS PERFORMED AS DIRECTED BY LBO, LLC AND AS NOTED ABOVE. THIS PLAT MEETS THE REQUIREMENTS OF 27 VSA 1403.

THE COLONY at LINCOLN PEAK
LBO, LLC 9 LOT SUBDIVISION
SUGARBUSH ACCESS ROAD
WARREN, VERMONT
MAY 2007 (REVISED MARCH 2008)
 BY CHARLES GRENIER, CONSULTING ENGINEER, P.C.
 P.O. BOX 445 - WATERBURY, VERMONT 05676
 SCALE: 1"=50'



Warren Town Clerk received for file 4/24/08 at 11:54 AM attest: [Signature] Town Clerk



June 1, 2023

Dear Mr Pochop,

I am writing on behalf of the Mad River Valley Chamber of Commerce to express my wholehearted support for the grant application submitted by the Town of Warren to create Segment I of the Sugarbush Access Road Path to connect Inferno Road and Golf Course Road. This is a true collaborative project, made possible with support of the Town of Warren from the Mad River Valley Planning District, Mad River Path Association, and Sugarbush Resort. These entities have played a crucial role in spearheading this initiative, and I believe the project has the potential to bring significant benefits to our community, particularly in terms of pedestrian safety, accessibility, and overall quality of life.

The primary objective of this project is to create a safe walking path for over 200 residents of Sugarbush workforce housing, which is located along the proposed path. Currently, many of these individuals do not have private transportation and are forced to walk on the roadways, exposing themselves to potential hazards. By establishing a dedicated and well-designed walking path, we can provide a secure and convenient means of transportation for these residents. This not only enhances their safety but also improves their overall well-being by offering an accessible and sustainable mode of travel to their workplaces and other essential destinations.

Additionally, this project aims to provide safe access points to Mad Bus stops along the path. By integrating transportation infrastructure and ensuring convenient access to public transit facilities, we can promote the use of sustainable transportation options and reduce the reliance on private vehicles. This not only improves the overall connectivity of our community but also contributes to our collective efforts in reducing traffic congestion and minimizing our carbon footprint.

Furthermore, while the objective of providing pedestrian access for the residents of the Downstreet housing complex, located further down the Sugarbush Access road, may not be achieved in Phase I, it is reassuring to know that it is part of the long-term plan for Phases II and III. Extending the walking path to encompass the Downstreet housing complex will ensure that all residents in the area have safe and convenient pedestrian access to recreational areas, amenities, and transportation options. This inclusive

approach to community development highlights the commitment of the project team to create an equitable and connected environment for all.

The collaboration between the Town of Warren, Mad River Valley Planning District, Sugarbush Resort, and the Mad River Path Association demonstrates a shared vision of creating a thriving and sustainable community. This project aligns perfectly with our goals of enhancing pedestrian safety, promoting alternative transportation options, and improving the overall quality of life for our residents.

In conclusion, I strongly endorse the grant application submitted by the Town of Warren, Mad River Valley Planning District, and Sugarbush Resort for Phase I of the project. The creation of a safe walking path for the Sugarbush workforce housing residents, the provision of safe access points to Mad Bus stops, and the future plans to extend pedestrian access to the Downstreet housing complex are all invaluable components of this initiative. I kindly request your favorable consideration and support for this grant application.

Thank you for your time and attention to this matter. Should you require any additional information or have any questions, please do not hesitate to reach out to me.

Sincerely,



Eric Friedman



Eric Friedman

Executive Director | [Mad River Valley Chamber of Commerce](#)

9 Route 17 | PO Box 173 | Waitsfield VT 05673

eric@madrivervalley.com | 802.496.3409 | madrivervalley.com

